

# Fairfield Village Heritage Assessment

Prepared for Project Manager

Gilda Di Vincenzo

City of Darebin

21 November 2017

Adopted by Darebin City Council 18 December 2017



Heritage Intelligence Pty Ltd

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Photograph on Front Cover. Source. Heritage Intelligence Pty Ltd. December 2016

**Document versions**

Internal project no.	Version	Issued to	Date issued
2017	Draft Progress Report	Gilda Di Vincenzo	27 February 2017
	Fairfield Village Heritage Assessment Progress Report Stage One	Gilda Di Vincenzo	20 March 2017
2017	Draft Fairfield Village Heritage Assessment Report	Gilda Di Vincenzo	26 June 2017
2017	Fairfield Village Heritage Assessment Report 28th June 2017	Gilda Di Vincenzo	11 July 2017
2017	Fairfield Village Heritage Assessment Report 2 November 2017 Amended to include a clause regarding the station area.	Gilda Di Vincenzo	2 November 2017
2017	Amended drawings to include allowance for a height of 3.5 m for first floor additions. Amended status of 5-5A Railway Place.	Gilda Di Vincenzo	20 Nov 2017

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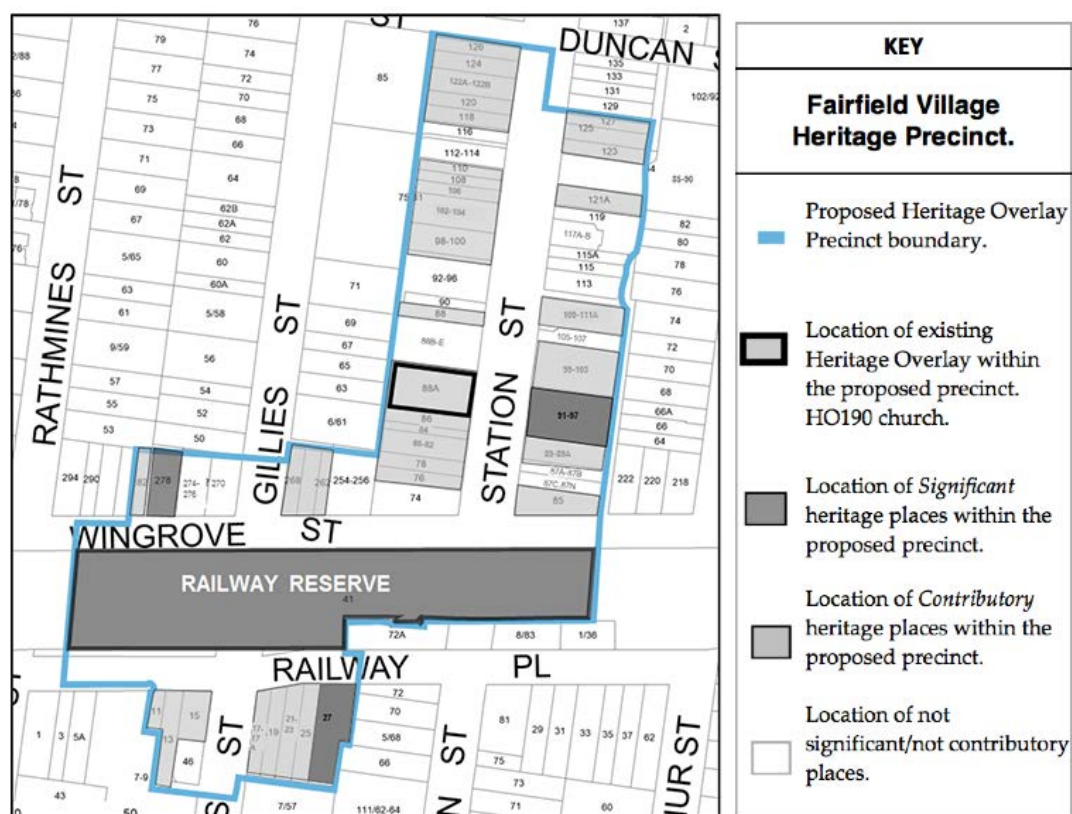


# Executive summary

## Introduction

The City of Darebin commissioned Heritage Intelligence Pty Ltd, to carry out the Fairfield Village Heritage Assessment ('Assessment') to identify those places that represent important themes of the municipality's history, as they relate to properties in the Fairfield Village Activity Centre and provide advice on appropriate design outcomes for future development in these locations. The study was undertaken in two stages. Stage One involved preliminary historical research and field surveys, identification of the proposed boundaries for the precinct, and individual places for detailed research and assessment, and a progress report with recommendations for Stage Two of the study. The historic research was benchmarked against the themes in the City of Darebin Thematic Environmental History (Context, August 2008). Stage Two involved detailed research for the precinct and individual places, documentation and assessment of one heritage precinct and three individual places, as well as developing detailed design advice, in consultation with Council's project manager, for future development within the precinct.

This report documents the methodology and findings of this Assessment. As a result of these findings, the following outcomes have been detailed in this report:- A Schedule of Heritage Places for the heritage precincts, citations for the Fairfield Village Heritage Precinct and two individually significant places, and development of planning scheme documents including an *City of Darebin Heritage Study Incorporated Plan - Permit Exemptions (2011) Recommended Amendments (2017)*, Heritage Overlay Schedule, and map of the precinct. Advice has also been provided, with twelve architectural plans and elevations, for recommended design outcomes for future development within the boundary of the Fairfield Village Heritage Precinct. A larger image of the map is in Appendix 8.2.



## Key Findings

The key findings of the Assessment include :-

- One precinct called Fairfield Village Heritage Precinct with 90 places of which 60 are heritage places has been researched and documented with a Statement of Significance and mapped and recommended permit exemptions.
- The precinct provides the opportunity for future development to enhance the contextual relationship and view lines of the heritage places within the streetscape and the 'village square' and thus strengthen the 'village feel'. The 'village square' is the area formed by the Fairfield Railway Station complex, railway reserve and adjacent streets which acts as a open square similar to those in European cities. ( See pages 83-84 and Figure 5.2 for clarification).
- An illustrated Schedule of Places includes identification of the level of significance with thresholds of Significant, Contributory or Not Significant for all 90 places in the precinct.
- The illustrated Schedule of Places also includes identification of the heritage features for each of the 60 heritage places in the precinct.
- Three key historical themes based on Council's Environmental History (Context 2008) are applicable to the Fairfield Village Heritage Precinct:- 'Peopling Fairfield', 'Transport and Communications' and 'Developing Fairfield's Economy'.
- Design Guidelines including twelve architectural drawings, provide measures which encourage the sensitive redevelopment of places within the proposed Fairfield Village Heritage Precinct.
- Detailed citations with a Statement of Significance, for two individual places outside the precinct at 5-5A Railway Place and 85-87 Gillies St. 5-5A was recommended for an individual Heritage Overlay, however, additional evidence regarding the authenticity of the fabric was provided to the consultants ( See Appendix 8.8) and it has been reassessed. It does not reach the threshold for an individual HO. A third place ( 278 Wingrove St) was researched and documented but as it was within the boundary of the precinct, and the research indicates that it does not require different HO controls than those in the precinct, an individual HO is not recommended.
- Both the precinct and one individual place (85-87 Gillies St) outside the precinct are of Local Significance and they are recommended for inclusion on the Heritage Overlay.
- There are two existing individual Heritage Overlay's within the proposed precinct. They are HO190 an 1885 church, and HO106 c1911 Railway structures. These places have an important role in representing key components of the history and fabric of the precinct, however, it is recommended that the Church should retain its individual HO, with interior controls. Whereas, it is recommended that all the historic the railway buildings and associated infrastructure ( see details in the Schedule) are best protected within the heritage overlay for the heritage precinct (with tree controls) as they are interwoven with the historic and aesthetic development of the precinct.
- The status of existing places nearby with a HO, but located outside the study area were considered during the study, but as they are not specifically related to the themes of the precinct, nor are they visually connected to it, their existing HO status is deemed to provide the most appropriate heritage protection for those properties.
- There are no places recommended for inclusion on the Victorian Heritage Register, or the Victorian Heritage Inventory.
- There are no recommendations for further assessment of places and precincts in the study area.

Map showing the boundary (blue line) of the Fairfield Village Heritage Precinct and the heritage places shaded in dark and light grey within the boundary.

## Recommendations

The study makes the following recommendations in order to implement the key findings of the final assessment.

### Adoption of Study

It is recommended that Darebin City Council formally adopts the Fairfield Village Heritage Assessment 2017.

### Implementation of Study

#### Planning scheme amendment

It is recommended that Darebin City Council implements the findings of this report by preparing an amendment to the Darebin Planning Scheme that will:

- Add one new place 85-87 Gillies St, assessed as being of local significance as an individual place to the Heritage Overlay of the Darebin Planning Scheme. The extent of registration is the whole of the property boundary.
- Add the Fairfield Village Heritage Precinct assessed as being of local significance as an individual place to the Heritage Overlay of the Darebin Planning Scheme. The extent of registration is the whole of the precinct as shown in the precinct plan. The precinct identifies the Significant, Contributory and not-significant/non-contributory places.
- Make changes to the Municipal Strategic Statement and the Local Planning Policy Framework, as required.
- Add the incorporated document *City of Darebin Heritage Study Incorporated Plan - Permit Exemptions (2011) Recommended Amendments (2017)*.
- Update HO190 in the Schedule to the HO of the Darebin Planning Scheme to include interior controls for the 1885 church.
- Remove HO106 from the Schedule to the HO of the Darebin Planning Scheme.
- Enter the individual place and one precinct into the Hermes Database using the documentation in this report and update HO190 with the interior controls and removal of HO106 from the Hermes Database.
- Make the final Assessment publicly available for download on Council's website.

### Acknowledgements

The contributions of the following people to the Fairfield Village Heritage Assessment are gratefully acknowledged: Gilda Di Vincenzo, Project Manager, City of Darebin.

# 1. Introduction

## 1.1. Background

The City of Darebin commissioned Heritage Intelligence Pty Ltd, to carry out the Fairfield Village Heritage Assessment ('Assessment') of selected properties in the Fairfield Village Activity Centre and provide advice on appropriate design outcomes for future development in these locations. The study was undertaken in two stages.

This report documents the methodology and findings of this Assessment. As a result of these findings, the following outcomes have been detailed in this report:- A Schedule of Heritage Places for the heritage precincts, citations for the Fairfield Village Heritage Precinct and two individually significant places, and development of planning scheme documents including an Incorporated Plan for Permit Exemptions, Heritage Overlay Schedule, and map of the precinct. Advice has also been provided, with twelve architectural plans and elevations, with recommended design outcomes for future development within the boundary of the Fairfield Village Heritage Precinct.

The terms used throughout this report are consistent with *The Burra Charter: The Australia ICOMOS Charter for Places of Cultural Heritage Significance* (2013). Assessment of the historic places and precinct uses the Burra Charter values of historic, social, aesthetic and scientific significance. The historic and social significance of the locality relative to the City of Darebin is primarily based on the history available in the specified previous heritage studies. The individual places and precinct were benchmarked against the relevant evidence, themes and conclusions in the City of Darebin Thematic Environmental History (Context, August 2008). Aesthetic and architectural assessment was primarily based on the aerial view, street view and physical fabric of the places and historical photos that were available.

## 1.2. Purpose of the Heritage Assessment

The purpose of the Assessment is to protect the 'village feel' of the commercial centre, by identifying the heritage places which contribute to the 'village feel' and recommend them for protection with a Heritage Overlay and secondly, to provide design guidelines to facilitate development within the heritage precinct which maintains the 'village feel'.

The consultants formulated criteria (Figure 2.2) and thresholds to determine the heritage places within Council's study area. The study area included parts of Station Street and parts of Wingrove Street and Railway Place with scope to include other nearby properties. Council provided a map showing the approximate boundary of the assessment area with 'valued street facades' marked in blue. (Figure 2.1)

**Existing Heritage Overlays** within and near the study boundary, were also identified by Council and their contribution to the proposed heritage precinct was assessed in this study.

The first stage of this Assessment comprised site inspections, preliminary historical research, identification of proposed heritage overlay boundaries and advice on appropriate design outcomes for future needs and development of the heritage places.

The second stage of the Assessment comprised detailed historical research on the proposed precinct and three individual places, detailed assessment of the physical characteristics of the heritage places within the precinct and three individual places, completed citations for the precinct and 3 individual places, refining and mapping the precinct boundary, refining the design advice and drawings for future development within the precinct, and development of planning scheme documents including an Incorporated Plan for Permit Exemptions, Heritage Overlay Schedule, and map of the precinct.

As defined in *The Burra Charter: The Australia ICOMOS Charter for Places of Cultural Significance* (2013:1):

*Places of cultural significance enrich people's lives, often providing a deep and inspirational sense of connection to community and landscape, to the past and to lived experiences. They are historical records, that are important expressions of Australian identity and experience. Places of cultural significance reflect the diversity of our communities, telling us about who we are and the past that has formed us and the Australian landscape. They are irreplaceable and precious. These places of cultural significance must be conserved for present and future generations in accordance with the principle of inter-generational equity.*

*Article 6.1 The cultural significance of a place and other issues affecting its future are best understood by a sequence of collecting and analysing information before making decisions. Understanding cultural significance comes first, then development of policy and finally management of the place in accordance with the policy. This is the Burra Charter Process.*

### 1.3. Assessment Outcomes

The Fairfield Village Heritage Assessment comprises one volume :-

- One precinct called Fairfield Village Heritage Precinct with 90 places of which 60 are heritage places has been researched and documented with a Statement of Significance and mapped and recommended permit exemptions. The precinct provides the opportunity for future development to enhance the contextual relationship and view lines of the heritage places within the streetscapes and 'village square'. The area bounded by the Fairfield Railway Station, railway reserve and adjacent streets acts as a 'village square' and thus strengthens the 'village feel'. ( See pages 83-84 and Figure 5.2 for clarification).
- An illustrated Schedule of Places includes identification of the level of significance with thresholds of Significant, Contributory or Not Significant for all 90 places in the precinct.
- The illustrated Schedule of Places also includes identification of the heritage features for each of the 60 heritage places in the precinct.
- Detained historic evidence and analysis of the three key historical themes drawn from Council's Environmental History (Context 2008) which are applicable to the Fairfield Village Heritage Precinct:- 'Peopling Fairfield', 'Transport and Communications' and 'Developing Fairfield's Economy'.
- Design Guidelines including twelve architectural drawings, provide measures which encourage the sensitive redevelopment of places within the proposed Fairfield Village Heritage Precinct. The contribution of views to, from and through the railway reserve, especially to Fairfield railway buildings, signal box, pedestrian bridge, palms and surrounds, in the proposed heritage precinct are noted to be of particular importance. There are 90 places in the precinct, of which 60 are heritage places and three of these have an individual heritage overlay.

- Detailed citations with a Statement of Significance, for two individual places outside the precinct at 5-5A Railway Place and 85-87 Gillies Street. 5-5A was recommended for an individual Heritage Overlay, however, additional evidence regarding the authenticity of the fabric was provided to the consultants and it has been reassessed. It does not reach the threshold for an individual HO. A third place (278 Wingrove St) was researched and documented but as it was within the boundary of the precinct, and the research indicates that it does not require different HO controls than those in the precinct, an individual HO is not recommended.
- Both the precinct and one individual place (85-87 Gillies St) outside the precinct are of Local Significance and they are recommended for inclusion on the Heritage Overlay.
- There are two existing individual Heritage Overlay's within the proposed precinct. They are HO190 an 1885 church, and HO106 c1911 Railway structures. These places have an important role in representing key components of the history and fabric of the precinct, however, it is recommended that the Church should retain its individual HO, with interior controls. Whereas, it is recommended that all the historic the railway buildings and associated infrastructure (see details in the Schedule) are best protected within the heritage overlay for the heritage precinct (with tree controls) as they are interwoven with the historic and aesthetic development of the precinct.
- The status of existing places nearby with a HO, but located outside the study area were considered during the study, but as they are not specifically related to the themes of the precinct, nor are they visually connected to it, their existing HO status is deemed to provide the most appropriate heritage protection for those properties.



## 2. Methodology

### 2.1. Introduction

The Study was prepared in accordance with the *The Burra Charter: The Australia ICOMOS Charter for Places of Cultural Significance* (2013) and its Practice Notes, and Planning Practice Note 1 *Applying the Heritage Overlay* (July 2015). The area for investigation is discussed in Section 2.2.1 of this report.

### 2.2. Project Management Plan

The study was undertaken in two stages. Stages 1 and 2 have been completed according to the project management plan agreed by Council's Project Manager at the inception meeting, and adapted when required and agreed, as new data informed the process.

This section outlines the methodology of this Assessment. The first stage of this Assessment comprised site inspections, preliminary historical research, identification of proposed heritage overlay boundaries and advice on appropriate design outcomes for future needs and development of the heritage places.

The second stage of the Assessment comprised detailed historical research on the proposed precinct and three individual places, detailed assessment of the physical characteristics of the heritage places within the precinct and three individual places, completed citations for the precinct and 3 individual places, refining and mapping the precinct boundary, refining the design advice and drawings for future development within the precinct, and development of planning scheme documents including an Incorporated Plan for Permit Exemptions, Heritage Overlay Schedule, and map of the precinct.

#### 2.2.1. Study Area

The area for investigation was generally defined by the Commercial 1 zoned land, comprising the main retail centre along Station Street north of the railway line and sections of Wingrove Street and Railway Place north and south of the railway line.

Definition of 'valued street facades' in the Commercial 1 zone, denotes buildings which, individually or as a cluster, are considered to add particular value and character to the streetscape due to their form and/or detailing. Sensitive redevelopment of sites containing 'valued street facades' was desired through the retention of these facades, appropriate setbacks of additional levels and use of respectful design elements and materials. The study brief also included scope to identify other nearby properties. The project manager specifically included the Uniting Church in Gillies Street. The railway reserve and its historic fabric, buildings, and views have been identified as significant contributors to the 'village feel' of the area. See Figure 5.2.



**Figure. 2.1** Study Map. Source. City of Darebin March 2017.

## 2.2.2. Criteria

The criteria applied to the places within the study area is shown in Fig 2.2. The consultants compiled these criteria in order to establish and prioritise, with the aim to assess the historic places that best represent the important historic themes of the Fairfield Commercial area. Heritage places that were documented, include, one or more of the following criteria:

Level of priority		Criteria
High	1	Places that have a prima facie case for rarity, aesthetic or architectural significance whose extant fabric is suitable for planning scheme protection.
	2	Places that have a high level of integrity and intactness.
	3	Places that represent important historic themes or activities.
	4	Places that represent historically important types (e.g. existing or previous shops, churches, hotels, streetscapes, transport, etc.)
Low	5	Places that represent important historic eras in the development of the Fairfield commercial area. eg Interwar period.

**Figure 2.2.** Heritage Intelligence Criteria

The Statements of Significance prepared for places in the Study were assessed using the *The Burra Charter: The Australia ICOMOS Charter for Places of Cultural Significance* (2013) definition of cultural significance, defined by their aesthetic, historic, scientific, social or spiritual values (see Appendix 8.5). Places may have a range of values applied. Statements of Significance were also assessed against HERCON Criteria (2008), as listed in Appendix 8.4.

## 2.3. Levels of Significance and Thresholds

Levels of significance are based on the three levels of government controls: National, State and Local, and thresholds are: Significant, Contributory or Not significant/Non-contributory.

### 2.3.1. Integrity and levels of significance of individual places.

**Individual Heritage Overlay:** The level of integrity/intactness for a place to reach the threshold for an individual Heritage Overlay, must be high, and have a prima facie case for rarity, aesthetic or architectural significance within the locality of the study area, whose extant fabric is suitable for planning scheme protection.

Both of the individually significant heritage places recommended for a Heritage Overlay (HO) in this Study are of local significance, and they are located outside the heritage precinct. 'Local Significance' includes those places that are important to a particular community or locality (Planning Practice Note 1).

The levels of significance generally accepted for use by heritage professionals today, are International, National, State and Local, as these relate to the corresponding government level responsible for protection of these places of cultural significance. However, within each of these thresholds there are likely

to be variations. For example, a place of Local significance may be significant to the 'local' community of a particular town or area or historic theme within the Fairfield Village while in other instances, a place may be significant to the whole of City of Darebin or a City-wide historic theme. In both cases the place would be considered to be of Local significance. Furthermore, with some complex places, an item such as a bell tower and bell in the grounds of a church would also be considered to be of local significance to the church complex, whereas, if it was located out of context in someone's garage, it would not reach the threshold for protection with a HO for an individual place.

### 2.3.2. Integrity and levels of significance within the precinct.

All heritage places have had some alterations over time, and all heritage places protected with a HO will be altered in the future, albeit through the planning process to ensure the heritage values are balanced and protected with the purpose of the changes. All places within the Fairfield Village Heritage Precinct are assessed against the precinct's three main historic themes: 'Peopling Fairfield', 'Transport and Communications' and 'Developing Fairfield's Economy'. These historic themes are expressed in the physical fabric of the precinct which is predominantly made up of interwar era commercial buildings and the spacious Federation era railway reserve with original buildings, palm trees and views. (See Figure 5.2)

Many early photographs of the places were found and these were analysed against the current status of the places. Together with the physical analysis these historic photos helped to determine the level of integrity. The degree of change and type of change were cross-checked against the history, to determine if the changes in themselves represented a significant historic event of that place.

Some alterations, whilst not ideal for a heritage place, may be easily reversible when an appropriate time arises, and the heritage place restored and enhanced.

The following definitions are consistent with the definitions used in previous heritage studies in the City of Darebin.

#### DEFINITIONS

Heritage Place	Significant	A <i>Significant</i> place is a single heritage place that has cultural heritage significance which may be independent of its context. These places may also contribute to the significance of a heritage precinct. <i>Significant</i> places within a heritage precinct will not usually have a separate Statement of Significance.
	Contributory	A <i>Contributory</i> place contributes to the significance of a heritage precinct, but would not be significant on their own.
	Significant feature	A <i>Significant feature</i> is any feature (building, tree, structure etc.) that the relevant <i>Fairfield Village Heritage Assessment</i> (2017) identifies as contributing to the significance of a heritage place or precinct. <i>Significant features</i> are described in the statement of significance for the place or precinct and in the Schedule in the <i>Fairfield Village Heritage Assessment</i> (2017).
Non Heritage Place	Non-contributory or Not Significant	<i>Non-contributory or Not Significant</i> places do not contribute to the significance of a heritage precinct. In some instances, a <i>Significant</i> place may be considered <i>Non-contributory or Not Significant</i> within a precinct. For example, an important Modernist house within a Victorian era precinct.
Maintenance		<i>Maintenance</i> means the continuous protective care of a <i>place</i> , and its <i>setting</i> . Maintenance is to be distinguished from repair which involves <i>restoration or reconstruction</i> .

## 2.4. Fieldwork

The fieldwork comprised site inspections and photographic documentation of all places within the study area, as seen from the street. The site visits identified the current condition and integrity of each place. The importance of the property relative to additional elements such as skylines and view lines that were of potential heritage significance were also noted and photographed.

A photographic schedule of places, with addresses, has been prepared and included in this report. The street numbers of each place were identified by, comparing the aerial image with an overlay of allotments and street numbers, and comparing our photographs of the street elevation and google street view. The addresses in the photographic schedule have been checked by Council's project manager against Council data as well as site visits to check numbers on doors, and they are considered to be accurate. Nevertheless, the photograph identifies the property being assessed for this study.

If a place within the study area clearly did not meet the necessary level of integrity as defined in the criteria, they were not included in the schedule of heritage places ( unless they are within a proposed heritage precinct.)

The 'Not significant' places that sit within the proposed heritage precinct(s) are included in the schedule and identified as a 'Not significant' place in that schedule.

While conducting the fieldwork, the consultants identified a small number of other places that had a heritage value, that were not identified in the 'valued street facades' in the City of Darebin Study Brief.

## 2.5. Historical Research

The aim of the historical research was to determine if the place had any significant associations to events or people, the purpose of the place, and the chronology of development on the site (including elements such as fences, trees and plantings) throughout its history.

The historic themes associated with each individual place and precinct were drawn from the City of Darebin Thematic Environmental History (Context 2008). Additional research in Stage 2 included sources such as Certificates of Title (where appropriate), Trove, and collections held by the State Library of Victoria, the National Trust and Public Records Office of Victoria, to determine who designed or built the place, the built date (where possible) and its association with any historic events or figures. This additional level of research was carried out for three individual places, but it was not carried out for all places within a precinct.

Key sources consulted included the following.

### 2.5.1. Key resources

- Darebin Heritage Study 2007
  - Thematic Environmental History (Context 2008)
  - Key findings and Recommendations (2008)
  - Review of 2000 Ward Study and Additional Stage 2 Place Assessments (2010)
- Fairfield Station Urban Design Strategy- Heritage Assessment Report (Context 1995)
- Local Historical Societies
- Darebin Heritage website, Darebin Library/City of Darebin, [heritage.darebinlibraries.vic.gov.au](http://heritage.darebinlibraries.vic.gov.au)
- Local history Facebook pages.
- Refer to the historic references for the precinct and individual places for more detail.

## 2.6. Documentation

As a result of the fieldwork and historical research during Stage 1 of the Study, the places to be documented comprised three individual places and one precinct in Stage 2 of the Study. The documentation of each place comprised the following elements.

Individual place histories were prepared for each of the three places of potential significance and the precinct, following the historical research, which included historic photos of the place where possible. Also compiled was a biography of the architect of the Uniting Church in Gillies Street.

A description was completed for each heritage place, noting the current condition and integrity, as well as identifying the layout of the site or property and any important elements of the setting such as outbuildings, fences, trees, plantings or other structures. The description for the places within the precinct was confined to the heritage features of the heritage places. A more detailed description of the individually significant places was provided. Photographs were provided in the individual citations, and cross referenced to the description, as a visual illustration of the place and its fabric.

A concise comparative analysis established the individual places and the precinct within the context of the City of Darebin, against buildings and precincts of similar type. This indicated whether the place and its components were common, unique or rare within the City. The comparative analysis was informed by the existing Heritage Overlay Schedule and the Victorian Heritage Database. While the comparative analysis has compared the building or structure architecturally to others within City, it must be recognised that although they may be of less architectural significance than a comparable place within the City, they remain of very high historical and social significance to the local Fairfield community and architecturally representative of the Fairfield Village.

## 2.7. Statement of Significance

As a result of the evidence provided by the documentation, a total of 1 precinct and 1 individual place were found to meet the threshold of cultural significance for an individually significant place, and a Statement of Significance was prepared for each. The statements are a concise summary of the cultural significance of the place. The *Burra Charter* provides the following definition for a Statement of Significance:

*A statement setting out what is important about a heritage place, where the significance lies and why it is important. Statements of significance should describe clearly all elements of a place which are assessed as contributing to its heritage significance and where appropriate should identify any buildings or other elements with a different basis of significance which may be included in the precinct whether that significance is to the State or to the locality.*

The following is a definition provided by Ian Wight in a 'Review of Criteria and Thresholds Discussion Paper' (2006):

*A Statement of Significance is about a physical place and therefore needs to make clear what part or parts of the physical place is significant. The Heritage Overlay is about regulating change to a physical environment, so a properly constructed Statement of Significance should provide just the sort of guidance that the planning authority needs.*

Thresholds and Criteria, informed the Statements of Significance which were compiled in accordance with *The Burra Charter* (2013) and the Planning Practice Note *Applying the Heritage Overlay* (July 2015), and evaluated against HERCON criteria (adopted 2008).

The Statements of Significance were categorised under the following headings:

***What is significant?***

This identifies what elements are significant (and from what building phases), as a guide to decision makers. For example, building, interiors, outbuildings, plantings, trees, fences. Mention is also made of elements that are not significant.

***How is it significant?***

This identified why it is important in terms of its aesthetic, historic, scientific, social or spiritual values, and at what level of significance (local, state, national).

***Why is it significant?***

This section elaborates on the values that make the place significant, as listed in 'How is it significant?'

## 2.8. Statutory Recommendations

Planning Scheme controls were identified for each place found to be of local individual heritage significance, in compliance with the Planning Practice Note *Applying the Heritage Overlay*. These controls are specified in the individual place citations.

For the places found to be of local individual significance, a draft Schedule to the Heritage Overlay was compiled, listing controls for each place. This document will form the basis of a future planning scheme amendment. (See Appendix 8.1)

Recommended amendments to the *City of Darebin Heritage Study Incorporated Plan - Permit Exemptions* (2011) have been made to so that it can be applied to the outcomes of this study. (A copy of *City of Darebin Heritage Study Incorporated Plan - Permit Exemptions* (2011) *Recommended Amendments* (2017), is in Appendix 8.2)

## 2.9. Mapping

A map that showed the extent of the recommended HO boundary, against an aerial image and the current title boundary, was provided for those places found to be of heritage significance located outside the heritage precinct.

A map of the boundary of the Fairfield Village Heritage Precinct, and the location of the heritage places within it, was prepared in a consistent format with existing heritage precincts in the City of Darebin planning scheme. (Section 5.1)

## 2.10. Hermes Database

Heritage Victoria's Hermes (Heritage Management Electronic System) Database is a depository of heritage information, accessed by local and state government, heritage professionals and other primary users of heritage data.

In accordance with the Planning Practice Note *Applying the Heritage Overlay*, it is recommended that the Hermes records be updated with new records for the three places recommended for a Heritage Overlay in this Study, so that the statement of significance



will be publicly viewable via the online Victorian Heritage Database when the place is included on the Heritage Overlay. The final citation information, including the Statement of Significance and a current photograph can be added to each Hermes record.

## 2.11. Design and Management Guidelines

Design Guidelines were prepared for the Fairfield Village Heritage Precinct primarily to guide new development within the precinct. Proposed heights, setbacks, architectural design techniques to use in new building in contemporary design are detailed in Section 4 of this report.

Management guidelines were also completed for each individually significant place recommended for the Heritage Overlay, to provide a succinct set of guidelines unique to each individual place, and to inform future management by the Council, Heritage Advisor, owners, committees of management and occupants. Heritage places will change with changes in ownership and use, the guidelines assist with the management of change so that “ the baby is not thrown out with the bathwater “ and future generations will be able to enjoy the culturally significant place too.

The Management Guidelines were based on the Conservation Principles of *The Burra Charter* (2013). A key principle, which was reflected in the guidelines, states that

*The Burra Charter advocates a cautious approach to change: do as much as necessary to care for the place and to make it useable, but otherwise change it as little as possible so that its cultural significance is retained.*

Whilst landowners are not obliged to undertake restoration works, the guidelines provide recommendations to facilitate the retention and enhancement of the culturally significant place, its fabric and its setting when restoration works or alterations to the building are proposed. They also identify issues particular to that place and provide further detailed advice where relevant. The guidelines addressed a combination of the following:

- **Setting** (views, fencing, landscaping, paths, trees, streetscape)
- **Additions and New Structures** (style, size, materials, reversible changes and preferred site and location shown in a blue polygon on an **aerial photo**)
- **Reconstruction and Restoration** (reconstructing known missing elements, removing accretions, type of rainwater goods, decorative finishes and elements, fences, verandahs)
- **Brick and Render Walls** (lime mortar, tuck pointing, paint colours, removal of paint, evaporation of damp and problem sealants)
- **Paint removal** (chemical removal benefits, damage from sand and water blasting, restoring early finishes and colours)
- **Signs** (where to locate them to reduce impact)

Whilst Council acknowledges and commends the good work that has previously been undertaken by the many owners of heritage buildings, the guidelines have been prepared to assist current and future owners of properties, who may not be aware of the heritage significance of a place, its fabric and setting or how to most effectively retain and enhance the historic features.

## 2.12. Study Constraints

As is usual for studies of this kind, the places and precincts were only investigated externally, and sometimes an inspection of an individual place was limited to the front and side view of the property only.

The site visits were carried out in December 2016. Since that time some could have been altered.

Detailed historical research was carried out for the heritage precinct and three individual places but not for the places located within the heritage precinct.

## 3. Assessment Findings

The fieldwork and preliminary historical research in Stage One identified the heritage places in the photographic schedule, a preliminary boundary of a heritage precinct and up to three individual places. Further historical research and detailed analysis of the physical fabric of the places was completed in Stage Two, and resulted in a defined boundary for the precinct, citations for the precinct and 2 individual places. The precinct and one individual place have been recommended for a Heritage Overlay.

### 3.1. Summary of Findings

The key findings of the Assessment include :-

- One precinct called Fairfield Village Heritage Precinct with 90 places of which 60 are heritage places has been researched and documented with a Statement of Significance and mapped and recommended permit exemptions.
- The precinct provides the opportunity for future development to enhance the contextual relationship and view lines of the heritage places within the streetscape and 'village square' and thus strengthen the 'village feel'. The area bounded by the Fairfield Railway Station, railway reserve and adjacent streets acts as a 'village square' and thus strengthens the 'village feel'. (See pages 83-84 and Figure 5.2 for clarification).
- An illustrated Schedule of Places includes identification of the level of significance with thresholds of Significant, Contributory or Not Significant for all 90 places in the precinct.
- The illustrated Schedule of Places also includes identification of the heritage features for each of the 60 heritage places in the precinct.
- Three key historical themes based on Council's Environmental History (Context 2008) are applicable to the Fairfield Village Heritage Precinct:- 'Peopling Fairfield', 'Transport and Communications' and 'Developing Fairfield's Economy'.
- Design Guidelines including twelve architectural drawings, provide measures which encourage the sensitive redevelopment of places within the proposed Fairfield Village Heritage Precinct.
- Detailed citations with a Statement of Significance, for two individual places outside the precinct at 5-5A Railway Place and 85-87 Gillies St. 5-5A was recommended for an individual Heritage Overlay, however, additional evidence regarding the authenticity of the fabric was provided to the consultants and it has been reassessed. It does not reach the threshold for an individual HO. A third place (278 Wingrove St) was researched and documented but as it was within the boundary of the precinct, and the research indicates that it does not require different HO controls than those in the precinct, an individual HO is not recommended.
- Both the precinct and one individual place (85-87 Gillies St) outside the precinct are of Local Significance and they are recommended for inclusion on the Heritage Overlay.

- There are two existing individual Heritage Overlay's within the proposed precinct. They are HO190 an 1885 church, and HO106 c1911 Railway structures. These places have an important role in representing key components of the history and fabric of the precinct, however, it is recommended that the Church should retain its individual HO, with interior controls. Whereas, it is recommended that all the historic the railway buildings and associated infrastructure ( see details in the Schedule) are best protected within the heritage overlay for the heritage precinct (with tree controls) as they are interwoven with the historic and aesthetic development of the precinct.
- The status of existing places nearby with a HO, but located outside the study area were considered during the study, but as they are not specifically related to the themes of the precinct, nor are they visually connected to it, their existing HO status is deemed to provide the most appropriate heritage protection for those properties.
- There are no places recommended for inclusion on the Victorian Heritage Register, or the Victorian Heritage Inventory.
- There are no recommendations for further assessment of places and precincts in the study area.

## 3.2. Historic Themes

The City of Darebin Heritage Study Thematic Environmental History (August 2008) was reviewed by the consultants to determine the relevant historic themes for the study area, resulting in the following three major themes.

1. Peopling Fairfield
2. Transport and Communications
3. Developing Fairfield's Economy

### 3.2.1. Peopling Fairfield

Although the land that comprises present day Fairfield was initially sold in the 1840's, the buyers were chiefly investors who didn't reside on their property. Whilst nearby suburbs progressed at a steady pace, the suburb of Fairfield took a while to get going. The apparent disinterest of speculators in their investments was possibly the onset of economic depression which dogged much of the 1840s. Some investors sold early but most realised little return during this time, apparently not even succeeding in acquiring tenants.

C H James, an entrepreneur and property speculator, created the Fairfield Park Estate from F Vidal's Crown Portion 114 (early in the 1880s). The Fairfield park subdivision was sold in generous suburban allotments at auction in November 1883 and then by successive 'extensions' to the subdivision from 1884 to at least November 1885 when extension 26 was sold (Library of Victoria, Fairfield Park no.26). The sales and the hype around the early auctions in particular, included in some cases a champagne luncheon held for the benefit of purchasers (Figure 3.1) (The Lorgnette, 25 September 1883, p. 2). This extravagance was typical of the property boom that preceded a crash in property

prices and an economic depression in the late nineteenth century (Context, 2008, p. 145 ). A railway had run through the estate from Clifton Hill to Alphington since 1883, but did not connect directly with Melbourne. The promise existed to extend this line to the then rural Heidelberg, which was realised in 1888 but ended in failure. To augment this directionless line, James had constructed a horse tramway from the Fairfield Park Station (later known at Fairfield Station), up Station Street for over a mile, which also ended in failure when it closed in 1890.



**Figure 3.1** Part of advertisement illustrating the 'suburban land sale of Mr C H James' Fairfield Park'. The Weekly Times n.d. (Darebin Heritage, Fairfield)

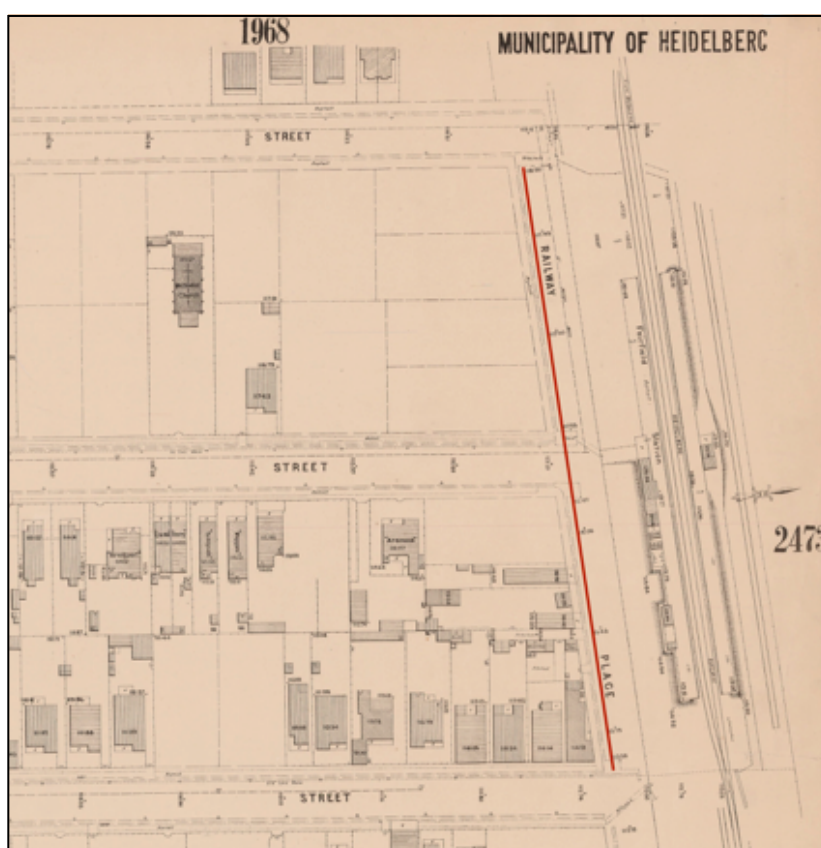
### 3.2.2. Developing Fairfield's Economy

Growth in Fairfield in the Victorian era was further stunted by the collapse of the economy in 1891 and the resulting great depression.

'All over Melbourne land companies were floated to buy land for quick resale at large profits, resulting in fortunes being made and lost. As land prices spiralled, banks and building societies over-reached their lending capacities and borrowers borrowed beyond their capacity to repay. Eventually, in December 1891 the whole structure began to collapse. People lost their savings, building stopped and housing estates remained vacant. The collapse resulted in a disastrous depression that brought unemployment and misery to many thousands, and halted further development for a decade or more.' (Context, 2008, pp 28-29)

As the economy recovered across the turn of the century (Context, 2012, pp 155 & 11), the improved railway services of the Heidelberg line which was opened in 1901 reduced travel time from Fairfield to Melbourne rendering Fairfield a more attractive residential option. Popular suburbs including Collingwood and Fitzroy were overcrowded and the population of Fairfield grew in response to 2,500 (Darebin Heritage, Fairfield). Repeated incidents of flooding continued to inhibit growth in Fairfield until the low areas were drained in 1907/1908 (Victorian Places, Fairfield).

Railway Place reflects the slow pace of the development of Fairfield pre WW1. Around 1912 there were only 5 properties directly fronting Railway Place in the precinct (Figure 3.2). Three of these between Gillies and Station Streets lie within the precinct boundaries.



**Figure 3.2** Plan c.1912 showing eastern frontage to Railway Place in the current Commercial 1 zoned land, highlighted by red line.

Source: Melbourne Metropolitan Board of Works Plan 2474, c 1912.



In 1921, the rail line was electrified and an article in the *Weekly Times* referred to Station Street and Railway Place Fairfield as "fast becoming busy thoroughfares...(with) many shops of the modern type." in contrast to the Fairfield of "not so many years ago which was almost a wilderness with unmade streets, a few houses here and there, and half a dozen shops. Although only five miles from the city, it was like living in the country." (Weekly Times, 1921, page 48)

The growth boom in Railway Place Fairfield post WW1 resulted from;

- draining of the lower parts of Fairfield making it more attractive for residential occupation thereby increasing customers (Darebin Heritage, Fairfield)
- making of Railway Place north and south (The Age, 10 February 1913, p.16)
- conversion to electrical rail service (1921) (Weekly Times, 25 June 1921, page 48, Trove)
- increased demand and overcrowding in suburbs more proximate to the city (Darebin Heritage, Fairfield)
- increased opportunities for local employment with the Australian Paper Mill using the old outer circle rail line, accessed from the junction at Fairfield Station, as a branch line (1919) (Darebin Heritage, Amcor, The Australian Paper Manufacturing Company) and growth at the Queens Memorial Hospital at the old Yarra Bend Asylum site (1917 and 1923) (Darebin Heritage, Fairfield Infectious Diseases Hospital)



**Figure 3.3** Showing buildings in Railway Place commercial area between Station Street and Gillies Street including the State Savings Bank (nearest to camera) and two adjoining buildings which may have been erected prior to 1912. Photographer unknown c. 1910-1930, Source State Library of Victoria.

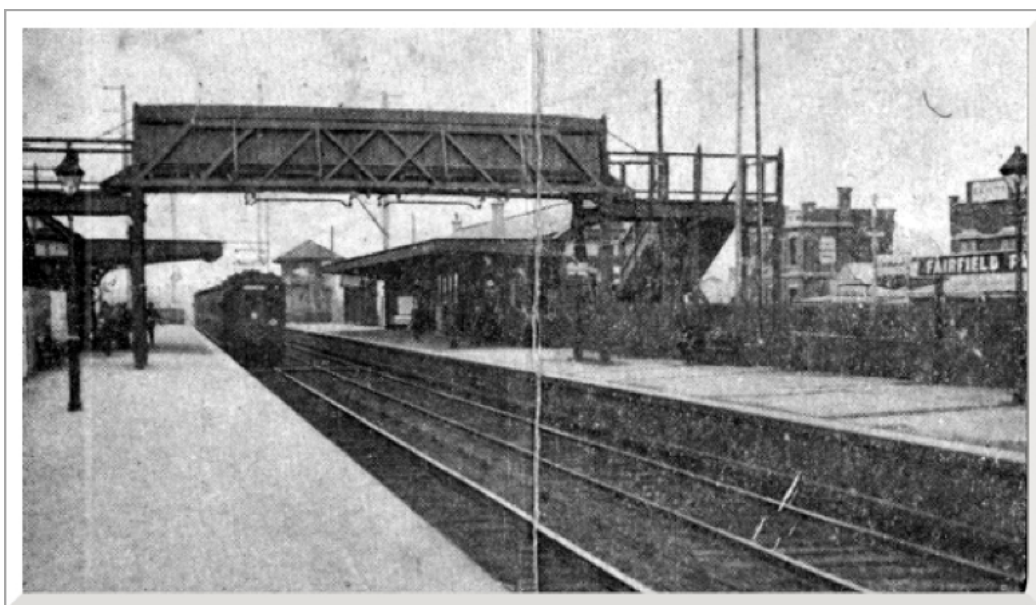
The electrification of the network in the early 1920s played an important role in the development boom that swept much of Darebin during the inter-war period.



Being immediately adjacent to the Fairfield Railway Station, the development of Railway Place followed the fortunes of the available transport options and related population expansion.

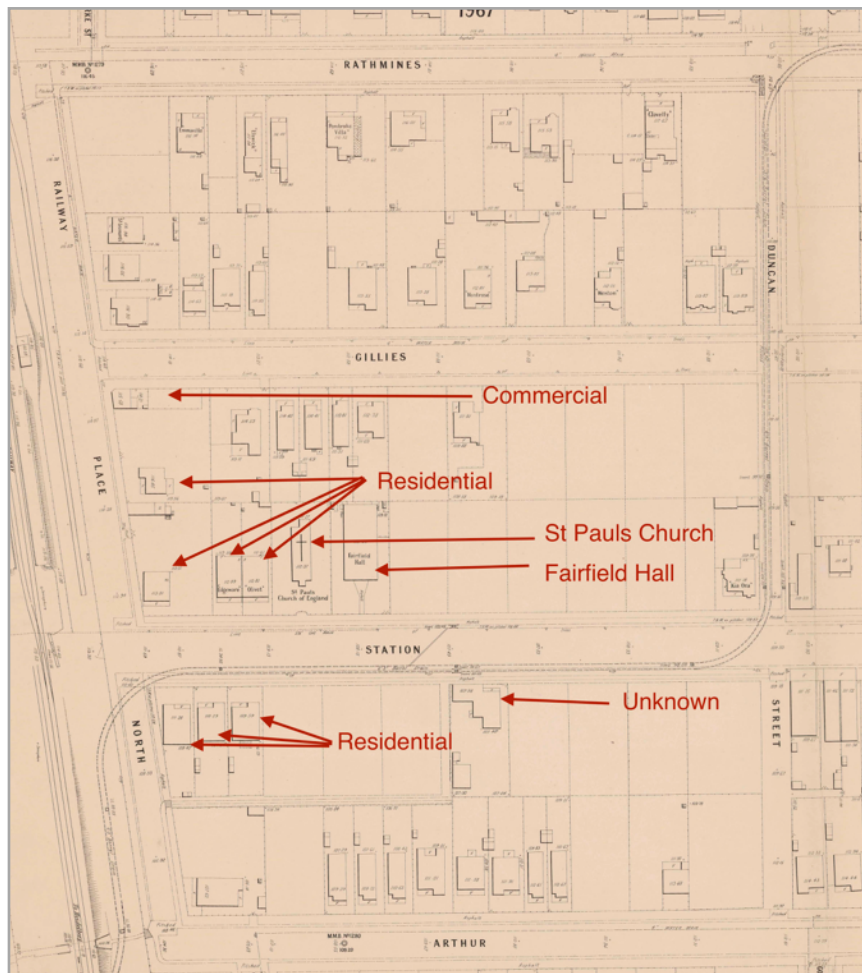
Railway Place remained central to community transport needs, and was therefore exposed to commercial traffic despite the inadequate nineteenth century train services. Options developed to supplement the limitations of the train service included buses (Context, 2008, pp 38 & 39) and "...a gas engine fitted to a carriage which operated briefly..." (Context, 2008, p. 35) until the superior service of the Heidelberg line opened in 1901 (Context, 2008, p. 36).

The importance of Railway Place as a thoroughfare meant that a pedestrian bridge was constructed to enable foot traffic across the railway line in March 1914 (Heidelberg News and Greensborough and Diamond Creek Chronicle, 14 March 1914, p. 2). (Figure 3.4)



**Figure 3.4** Showing view from Fairfield Park railway station platform showing pedestrian bridge linking Railway Place to Wingrove Street. Note signal box in the distance, shops and SSBank on Railway Place. photographer unknown n.d. Source (Darebin Heritage, Fairfield Railway Station).

The c 1912 Melbourne Metropolitan Board of Works detail plan 2473, (Figure 3.5) shows that there were only 11 properties (7 residential, 1 commercial, 1 church, 1 town hall and 1 unknown) directly fronting Station Street in the precinct. The development of the suburb in the late nineteenth and early twentieth century was illustrated by the building of community facilities such as the first St Paul's Anglican Church in Station Street, erected in 1885, which also served as the district's first school.



**Figure 3.5** Plan c.1912 showing developments in Station Street and Railway Place North ( now Wingrove St) in the current Commercial 1 zoned land,

Source: Melbourne Metropolitan Board of Works Plan 2474, c 1912



**Figure 3.6** View of Station Street, looking north from railway line c. 1920s, showing rapid growth since the 1912 MMBW plan above, note Bell Accord building on the right with first floor arched window openings. (Images of Yesteryear, Fairfield). Image supplied by Images of Yesteryear. Contact Images of Yesteryear for reproduction in any form.

In 1921 the Weekly Times credited the Fairfield community for their "mutual help and generous and active support (for) local affairs...(explaining) somewhat the rapid progress that is being made there." (Weekly Times, 25 June 1921, p. 48)

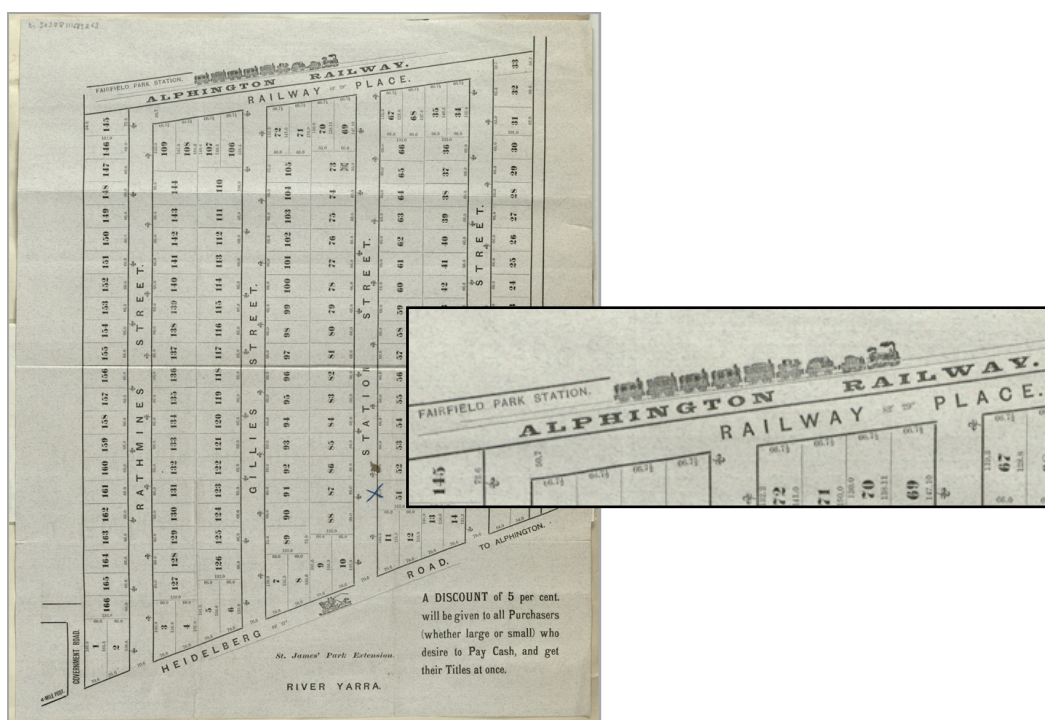


**Figure 3.7** c.1960s View of Station Street Fairfield looking south towards railway line, note the landmark quality of the large junction signal box to the right, in the distance and the absence of the street trees seen in the c1920 photo (Fig 3.6) Access to Station Street by car was made more appealing by its convenient access to the main arterial Heidelberg Road. Source Public Record Office Victoria, VPRS12800/P0001/H3176



### 3.2.3. Transport and Communication

Transport and the lack of it, is a pivotal theme in the development of Fairfield. The c1883 Plan of original subdivision of Fairfield Park Estate (Source SLV) shows the Fairfield Park station as a feature at the top of the map.



**Figure 3.8.** Plan of the original subdivision of Fairfield Park Estate, showing the Fairfield Park Station and a steam train (detail). Source: State Library of Victoria.

The present station buildings were erected by the Lang Brothers under a contract signed on 26th. April, 1911. An earlier signal box had been opened on 23rd. March, 1891 to coincide with the opening of the Outer Circle line, but this was replaced with the existing one in 1913. The station name was changed to Fairfield on 14th. November, 1943. The level crossing gates at Station Street and associated mechanism in the signal box were removed and replaced with automatic boom barriers on 2nd. November, 1969. The signal box remaining open for train movements in connection with the paper mill siding and for special workings after that time. It was finally closed on Monday, 28th. February, 2000 following the disconnection of the Australian Paper Manufacturers' siding. It is historically important as the earliest surviving signal box of its type in the metropolitan area and the equal oldest of its type in the State, comparing in this respect with the Creswick signal box.

The existing station building complex is historically significant as the passenger and goods station for its locality since construction in 1911, indirectly recalling its opening in 1888 and remaining in use for its original purpose at present. It is important also for its capacity to offer an insight into the important work of the chief architect's office following its establishment in 1908 when the Railways Department was engaged in the improvement of station facilities across the network in response to unprecedented traffic growth. Whilst this design compares closely with Montague (demolished) and loosely with nearby Westgarth, it appears not to be one of a number of similar stations, the

adoption of the Arts and Crafts mode, however, enabling it to be readily identified with the early work of the chief architect's office.

It is aesthetically important as an increasingly rare, substantially complete railway complex of the Federation era, the Arts and Crafts motifs including strap work, spandrels, fibro cement slates and rough cast work being hall marks of J. W. Hardy's office at the time. Situated in between Railway Place to the south, and Wingrove St to the north and at the southern entry to the Station Street shopping centre, the complex, complete with signal box, imparts character to the centre and has become a local icon, valued by the community.

During the inter-war period the Heidelberg line was electrified (1921) and the large numbers of inter-war style buildings in Railway Place and Station Streets reflect the significance this event to the economy of Fairfield. Thus the electrification of the railway reinforces the historic, aesthetic and social links of the interwar commercial buildings with the railway station.

The electrified railways sped up the journey to the city considerably and ran at more frequent intervals thus encouraging a new wave of suburban development (City of Darebin Heritage Study, Volume 1, Draft Thematic Environmental History, Stage 2 Report, August 2008, page 36). In response to the increase in population in the Darebin area, " new buildings were erected during the first decades of the twentieth century at the busiest stations including Fairfield, Merri, Northcote, Thornbury and Bell. " (City of Darebin Heritage Study, Volume 1, Draft Thematic Environmental History, Stage 2 Report, August 2008, page 36). The growth in Railway Place reflects this change, the street fronts being almost completely filled with commercial premises between Rathmines and Station streets (Figure 3.9), constructed in the inter war architectural styles. (Victorian Heritage Database, Fairfield Railway Station HO106).

In 1894 the Victorian Railways sought to encourage railway station staff to beautify their stations by instituting a prize for the most beautiful metropolitan and regional railway station (Mercury and Weekly Courier, 8 March 1894, p. 3 ). Before the prize was announced it appears that Fairfield Railway Station was already known for its attractive garden ornamentation (Mercury and Weekly Courier, 13 September 1894, p. 2) and in 1894 and 1895 they achieved second place and in 1896 won the competition (The Age, 12 June 1896, p. 4)

Two distinctive palms located at the front of the station facing Railway Place are typical garden features of the Federation era. The palms may therefore date from the same period of construction as the existing Federation era station buildings. It is also possible that the palms are the remains of plantings that formed part of the nineteenth century beautification program or that they date from around 1938 when some garden improvements are known to have been made at the Fairfield Railway Station (The Age, 6 October 1938, p.4). Whichever time they were planted the palms are the only remaining plantings from the early period of the Fairfield Railway Station which is now dominated by more recent plantings of gum trees.

Railway Place Fairfield evolved into a commercial centre based on the proximity of the location to the railway line and the requirement for a small commercial centre to serve a burgeoning population with limited resources for travel. The design of the strip reflects the trends of the inter war era, which time it experienced peak growth and the minor modifications since, reflect the continued development of the community of Fairfield.

The character of the Wingrove and Station Streets Commercial precinct as a thriving commercial centre has developed as a result of the initial size and orientation of the subdivision, limited initial development leaving vacant land ready for commercial occupation and the proximity of the streets to the Fairfield Park Railway Station and other public transport options. The contemporary reliance on cars and convenient car

access to Fairfield combined with an active community has enabled the precinct to become a shopping destination.



**Figure 3.9** Railway Place looking east from Rathmines Street towards Gillies Street and Station St. The signal box (1913) and station buildings (1911) are visible behind the railway fence. The State Savings Bank is visible in the distance fronting Railway Place, and several shops. Photographer unknown, n.d., source Railway Negatives Public Record Office Victoria VPRS12800/P0003/44

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**Note:** A history of street name changes.

Wingrove Street originally extended to the east from Arthur Street alongside the railway line. During this time Wingrove Street between Arthur Street and Rathmines Street was known as Railway Place North. Likewise for a period of time Railway Place was known as Railway Place South. Sometime since 1945 changes were made to the road alignment at the termination of Arthur Street. The modification connected Wingrove Street and Railway Place together as a single thoroughfare (currently connected by a roundabout) known as Wingrove Street and the names Railway Place North and Railway Place South ceased to be used.

### 3.3. Individual Heritage Overlays - Discussion

#### 3.3.1. Existing

There are two existing individual Heritage Overlay's within the study area. They are HO190 a church, and HO106 Railway structures. These places have an important role in representing key components of the history and fabric of the precinct, however, it is recommended that the Church should retain its individual HO, with interior controls added to the Schedule of the Heritage Overlay.

#### HO106

Further research and analysis during Stage 2 resolved that the individual HO106 on the Railway structures does not protect the heritage values of the Railway Reserve as it only protects, in isolation, the signal box, and actual station buildings, whereas the existing documentation by Fairfield Station Urban Design Strategy Heritage Assessment Report (Context 1995) and new documentation in this study concludes that significance includes the setting, the functional inter relationship of the various parts, as well as views within and across the Railway Reserve from Wingrove Street and Railway Place are vital to understand and enjoy the important heritage values. It is noted that the Railway Reserve and heritage features are recommended for inclusion in the new HO for the Fairfield Village Heritage Precinct (with tree controls to protect the palms), and to remove the individual HO106 from the Schedule to the Heritage Overlay.

The individual HOs located outside the study area were not located adjacent to the proposed precinct or visually connected to it and therefore deemed to provide adequate heritage protection for those properties.

#### 3.3.2. Detailed Citations

Five places were considered for further research in Stage Two of the study.

Two of those places were outside the proposed heritage precinct and they were assessed for an individual Heritage Overlay. They are 5-5A Railway Place and 85-87 Gillies Street. Detailed citations are included in this report.

**5-5A Railway Place** is a single storey Interwar shop, which is opposite the Railway Reserve, but visually separated from the other historic shops in Railway Place by a large development at 7-9 Railway Place (currently under construction). 5-5A was recommended for an individual Heritage Overlay, however, additional evidence regarding the authenticity of the fabric was provided to the consultants and it has been reassessed. It does not reach the threshold for an individual HO.

**85-87 Gillies Street** After discussion with the Project Manager, it was agreed that historical research and heritage assessment will also be undertaken for St Andrew's Uniting Church, 85-87 Gillies Street on the corner of Duncan St, as historically and architecturally worthy of an individual Heritage Overlay.

A third place that was also assessed is 278 Wingrove Street, which is within the proposed heritage precinct, but the architectural style and historic use are different to the commercial buildings within the precinct. But the recommendation is to include it as a significant place within the heritage precinct, rather than an individual HO. The detailed citation is included in this report.

**278 Wingrove Street** is a very intact Interwar Spanish Mission style building. It was built as a residence but was extended to include a Doctor's surgery at the front, and it is now used as a medical centre. Any future development on the site, is likely to be commercial, rather than as a stand alone residence and therefore it is

best protected with consistent Heritage Overlay controls within the precinct. Nevertheless, as the architectural style and historic use are different to other commercial buildings in the precinct, some primary research was carried out in Stage Two of the study.

The fourth and fifth places were also outside the heritage precinct, and they are 75 Station Street and 3 Railway Place. After consideration of their degree of intactness, they did not reach the threshold for a potentially individual Heritage Overlay and no detailed assessment was carried out.

**75 Station Street** is a two storey Interwar shop, which is intact at the first floor level but the ground floor shop front has been completely altered. The design is typical rather than exceptional in style, and therefore, it is not considered to reach the higher threshold for an individual heritage overlay. The site is visually separated from the other historic shops, by several large modern developments in Station Street and Railway Place, to be included in the precinct.

**3 Railway Place** is a single storey Interwar shop, which has been greatly altered below the verandah, leaving only the verandah and the parapet as the main intact heritage fabric. The design is typical rather than exceptional in style, and therefore, it is not considered to reach the higher threshold for an individual heritage overlay. The site is visually separated from the other historic shops, by a large modern development at 7-9 Railway Place, to be included in the precinct.

### 3.4. Heritage Precinct - Discussion

For the purpose of this study, a heritage precinct is considered to possess one or more of the following characteristics (Section 2.8.1 Review of Heritage Provisions in the Planning Schemes, Aug 2007) :

- They contain contributory places that individually or as a group illustrate important themes set out in the thematic history.
- The places within a precinct may or may not adjoin one another. Where they do not form a continuous grouping they will have a strong and demonstrated thematic association.
- Where places form a contiguous grouping they will have largely intact or visually cohesive streetscapes that are either aesthetically or historically significant (or both).
- Precincts that are historically significant will include elements such as housing styles and subdivision layouts that are representative or typical of a particular era or type.
- Precincts of aesthetic significance will be distinguished by the high or exceptional quality of the housing design and/or estate layout and features when compared to other examples.

The heritage precinct is made up of significant, contributory and not-significant, places.

The 'village feel' within the Fairfield commercial zone, is not conclusively defined in the project brief, however, there is a list of elements in the built environment that contribute to it, as well as known community views. This aspect is discussed in Section 6 Design Guidelines.

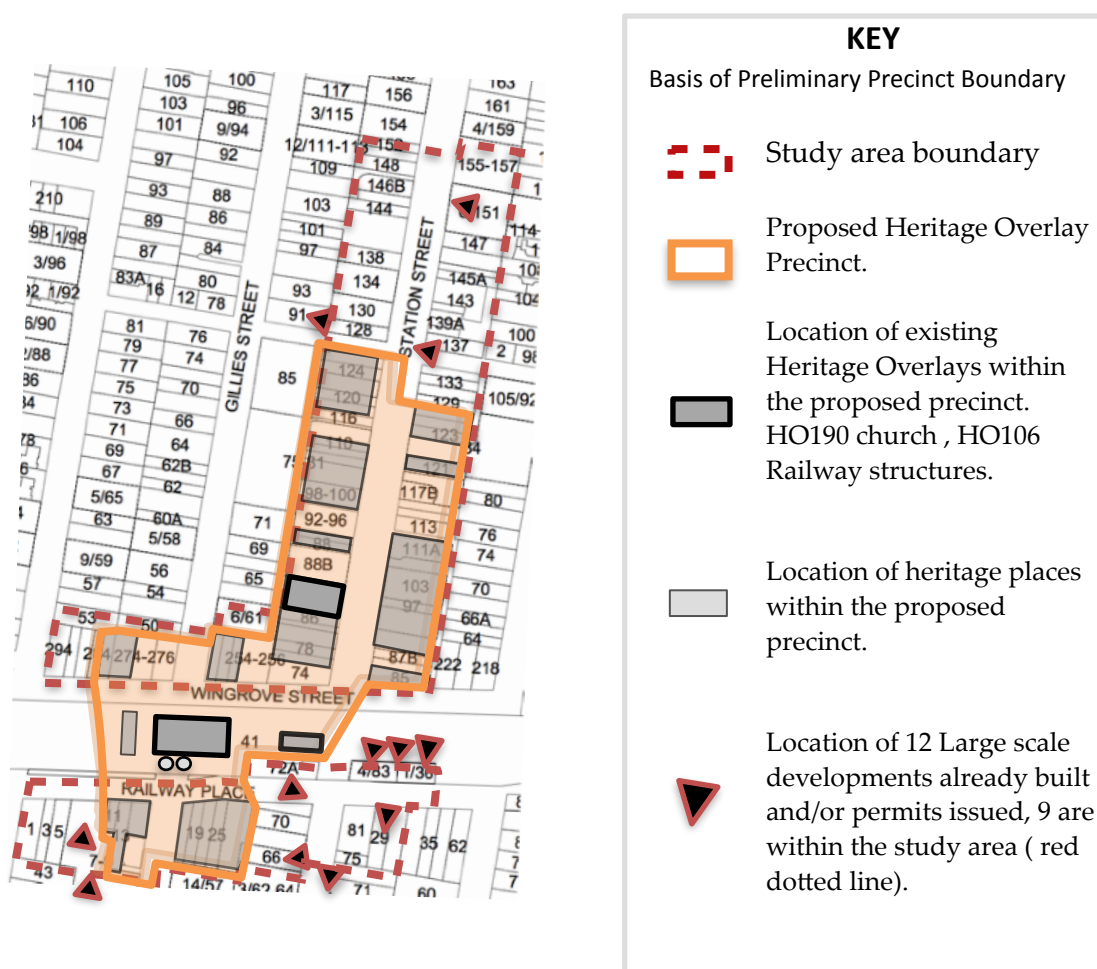
#### Preliminary Development of the Precinct Boundary.

The study boundary generally followed the boundary of the Commercial 1 zoned land, with scope for places nearby.

Both the historical research and fieldwork provided evidence that the open space of the railway reserve is a very large and strong element in the historical development of Fairfield

commercial village. It has retained many of the historic views of it within the proposed precinct. (Figure 3.10)

A preliminary precinct boundary was established at the end of Stage One. Figure 3.10 illustrates the basis of the preliminary boundary and further explanation follows in the text below it.



**Figure 3.10.** Map of the Stage One Preliminary Fairfield Village Heritage Precinct developed for further research and documentation in Stage Two.

The precinct boundary encloses the majority of the significant and contributory heritage places within the study boundary. There are four clusters of sites that are not included in the precinct. These are listed below and illustrated in the Preliminary Precinct Boundary developed at the end of the field work in Stage One:

- Station Street - north of Duncan St,
- Wingrove Street - the far west end,
- Railway Place - the far west end,
- Station Street - south of the railway tracks around the intersection of Railway Place.
- **Station Street - north of Duncan St.** The places north of Duncan Street are mostly 'not significant' and the very small number of heritage places north of Duncan Street are

visually disconnected from the large numbers of visually cohesive heritage places in the precinct. Furthermore, there are two large, wide and high developments, on the north side of Duncan Street, not yet built but have planning approval, which visually isolate the small number of heritage places north of them, and the heritage places do not meet the threshold for individual significance.

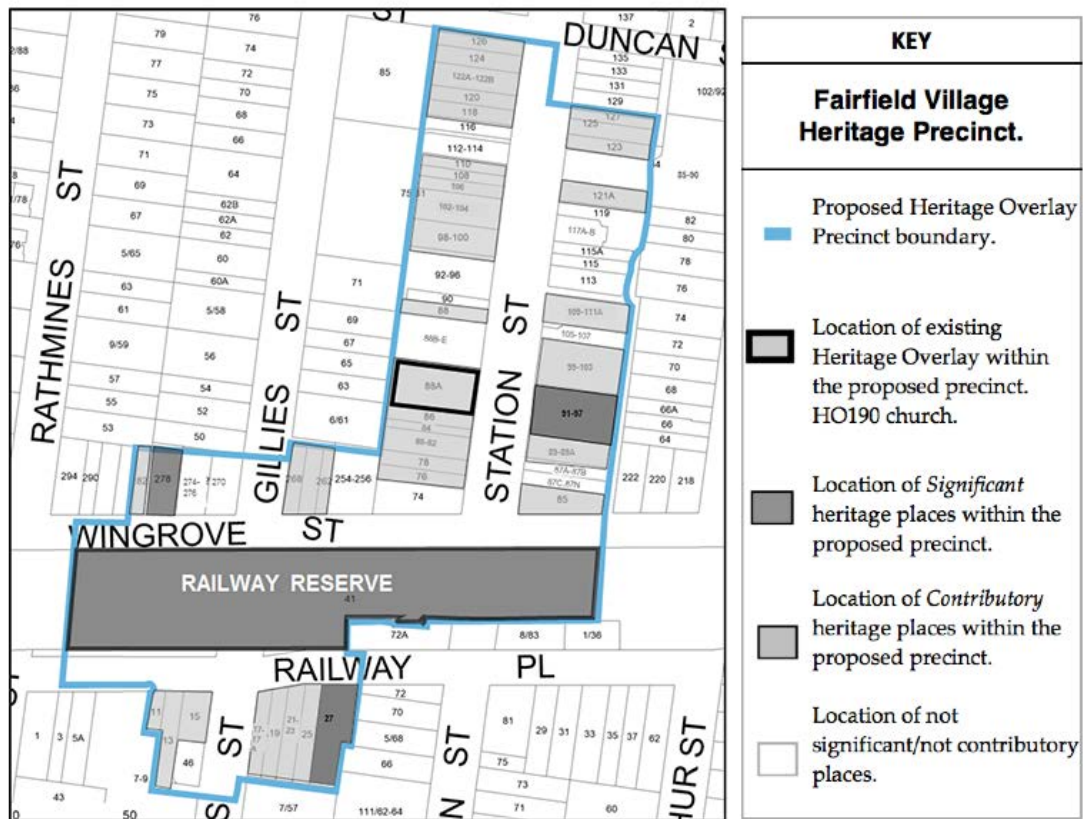
- **Wingrove Street - the far west end.** All the places west of 284 Wingrove St are either not historic or so altered they do not reach the threshold for inclusion in the precinct as a significant or contributory heritage place.
- **Railway Place - the far west end.** There are four places from 9 to 1 Railway Place, at the far west end which are not included in the precinct. 7-9 has been approved for a large building which is significantly out of character in scale and height, compared with the narrow, single and two storey historic places in Railway Place. Furthermore, the scale and design of the new development at 7-9 visually isolates the last three properties at the west end of Railway Place from the cluster of heritage places in this street, whereas, if the existing building at 7-9 ( which is 'not significant' but only single storey) was retained, the heritage precinct boundary would have extended to Rathmines St.
- **Station Street - south of the railway tracks around the intersection of Railway Place.** There are approximately 11 properties in this vicinity, but only two of them are heritage places, however, they not only do not meet the threshold for individual heritage overlay, they are isolated from the heritage precinct by several large modern developments, (four of these have planning permits but are not yet built). Had the development at 72A Station Street, adjacent to the very historic Signal Box, not been approved, the recommended boundary of the precinct would have been extended along both sides of Railway Place to include the corner sites at 72A and 72 Station St.

### Final Precinct Boundary

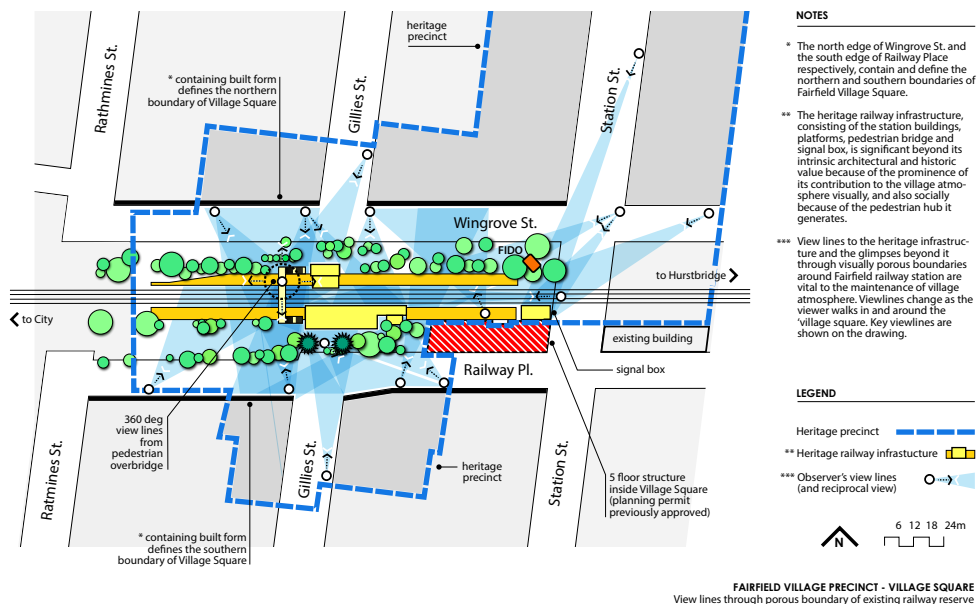
The Fairfield Village Heritage Precinct boundary continued to be developed and finalised in Stage Two, based on further analysis of the historical research, and the important view lines connecting the heritage places. It was resolved that the individual HO106 on the Railway structures does not protect the heritage values of the Railway Reserve as it only protects, in isolation, the signal box, and actual station buildings, whereas the existing documentation by Fairfield Station Urban Design Strategy Heritage Assessment Report (Context 1995) and new documentation in this study concludes that significance includes the setting, the functional inter relationship of the various parts, as well as views within and across the Railway Reserve from Wingrove Street and Railway Place are vital to understand and enjoy the important heritage values. It is noted that the Railway Reserve and heritage features are recommended for inclusion in the new HO for the Fairfield Village Heritage Precinct (with tree controls to protect the palms) , and to remove the individual HO106 from the Schedule to the Heritage Overlay.

There are 90 places in the precinct, of which 60 are significant or contributory heritage places. A detailed citation is in Section 5 of this report.





**Figure 3.11.** Map of the Fairfield Village Heritage Precinct developed and documented in Stage Two. A larger image of the map is in Appendix 8.2.



**Figure 3.12.** Detail diagram of the 'Village Square', Fairfield Village Heritage Precinct developed and documented in Stage Two.



## 4. Citations - Individual heritage places

Detailed citations have been written with a history, description, comparative analysis, management guidelines, Statement of Significance, and Heritage Overlay Schedule for two individual places outside the precinct.

- 5-5A Railway Place
- 85-87 Gillies St.

Only 85-87 Gillies St is of Local Significance and recommended for inclusion as an individual place on the Heritage Overlay.

A third place ( 278 Wingrove St) was researched and documented but as it was within the boundary of the precinct, and the research indicates that it does not require different HO controls than those in the precinct, an individual HO is not recommended. The citation for that place is included in the Precinct Citation.

## 4.1. 85 Gillies Street, Fairfield

### St Andrew's Alphington & Fairfield Uniting Church

<b>Locality:</b>	<b>FAIRFIELD</b>
<b>Place address:</b>	<b>85 GILLIES STREET</b>
<b>Citation date</b>	2017
<b>Place type (when built):</b>	St Andrew's Alphington and Fairfield Uniting Church
<b>Recommended heritage protection:</b>	Local government level: Yes Vic Heritage Register: No Heritage Inventory (Archaeological): No Local Planning Scheme: Yes
<b>Place name:</b>	<b>Fairfield Uniting Church</b>



<b>Architectural Style:</b>	Post-War Gothic
<b>Designer / Architect:</b>	Francis Bruce Kemp
<b>Construction Date/ Builder:</b>	1950 (church), Builder A. F. Larsen

## Statement of Significance

This statement of significance is based on the history, description and comparative analysis in this citation. The Criteria, A, B, C, D, E, F, G, H is the Heritage Council Criteria for assessing cultural heritage significance (HERCON). Heritage values of historic, social, aesthetic and scientific significance are based on the Burra Charter definitions. Level of Significance, Local, State, National, is in accordance with the level of Government legislation.

### *What is significant?*

St Andrew's Alphington and Fairfield Uniting Church at , 85 Gillies St, Fairfield, is significant. The original form, materials and details of the building as constructed in c1950 are significant. The Empire Honour Roll, Alphington Methodist Church (First World War) and St Andrew's Presbyterian Church Honour Roll (First World War), are also significant. Later buildings and alterations to the earlier building are not significant.

### *How is it significant?*

St Andrew's Alphington and Fairfield Uniting Church at , 85 Gillies St, Fairfield, is locally significant for its historical, social, spiritual and aesthetic values.

### *What is significant?*

St Andrew's Alphington and Fairfield Uniting Church at , 85 Gillies St, Fairfield is **historically significant at a local level** for its association with the pattern of development of the suburb of Fairfield. The Empire Honour Roll, Alphington Methodist Church (First World War) and St Andrew's Presbyterian Church Honour Roll (First World War), are also historically significant. The St Andrews Alphington Fairfield Uniting Church building is historically significant as an example of a surviving design by Victorian architect F Bruce Kemp a highly accomplished architect whose clients included churches and private schools, primarily those associated with the Presbyterian Church. (HERCON criteria A and H).

St Andrew's Alphington and Fairfield Uniting Church at , 85 Gillies St, Fairfield is **socially significant at a local level** for its long association with the communities of Alphington and Fairfield as a focus for ceremonies related to birth, deaths, and marriages and for community the continuous social and fundraising activities for the benefit of local and broader charities and the 'war effort' during WWII. (HERCON criterion G).

The St Andrews Alphington Fairfield Uniting Church building is of spiritual significance as a place of worship for the Presbyterian community of Alphington and Fairfield since 1952 and the site as a place of worship since 1915. (HERCON criterion G).

St Andrew's Alphington and Fairfield Uniting Church at , 85 Gillies St, Fairfield is **aesthetically significant at a local level** for the accomplished transitional modernist/traditionalist design which is, overall asymmetric, although the three gable ends, including the whole of the north elevation facing Duncan St, are symmetrical in design. The north facade is minimalist and modernist in design, whereas the west elevation facing Gillies St has a deeply sculptured design more typical of Gothic churches. The Arts and Crafts materials of earthy coloured clinker bricks and marseille terra cotta tiles dominate and unify the whole building. (HERCON criteria E).

## Statutory Recommendations

This place is recommended for inclusion in the Schedule to the Heritage Overlay of the City of Darebin Planning Scheme.

<b>External Paint Controls</b>	Yes
<b>Internal Alteration Controls</b>	Yes,
<b>Tree Controls</b>	No
<b>Outbuildings or fences which are not exempt under Clause 43.01-3</b>	No
<b>Prohibited Uses May Be Permitted</b>	No
<b>Incorporated Plan</b>	Yes
<b>Aboriginal Heritage Place</b>	Not assessed

## History

### Thematic context

This place is associated with the following themes identified in the City of Darebin Heritage Study Volume 1 Draft Thematic Environmental History Stage 2 Report (2008)

- 5. Building Suburban Darebin
  - 5.3 Developing the suburban ideal
- 7. Community and Culture
  - 7.1 Worshipping, founding churches.

### Place history

The denomination of St Andrew's Alphington and Fairfield Uniting Church resulted from the amalgamation of the Presbyterian, United and Methodist churches in 1977 (Uniting Care Australia, Our History). Before this time the church at this site was known as St Andrews Presbyterian Church and the crest of the Presbyterian Church is still visible in the gable end above the entry porch..

The foundation stone of the present St Andrew's Alphington and Fairfield Uniting Church, designed by architect F Bruce Kemp, was laid in 1950. (The Argus, 9 December 1950, p. 6) (Figures H1 and H6) Building works were completed in early September of 1952 and the new church was dedicated and opened on the 27th of the same month. (The Age 13 September 1952, p.16)

The current St Andrew's Alphington and Fairfield Uniting Church design (Figure H2) was modified from Kemp's original plan which included a small projecting bay at the north west corner of the building, and an integrated connection to the existing Kindergarten Hall, the nave was lengthened among other architectural variations. (Figure H3) The Church was paid for by a combination of community funds raised and a loan of L6500 from the Presbyterian Special Commission of

Assembly (The Age 10 November 1949, p. 2). The present building was located in almost the exact place on the block as the previous, most likely timber, building which had been burnt down by arson in 1948 (The Weekly Times 15 September 1948, p. 2, Trove).

When the original St Andrews Presbyterian Church was burnt down the complex of buildings at the site included a Church Hall (built in 1920/21 by community donation) (Public Building File PB7021A, PROV Victoria and Heidelberg News and Greensborough and Diamond Creek Chronicle, 4 September 1915, p. 2) and the Church building that had been relocated from Heidelberg Road in 1915. The Church is said to have been taken "...over the old outer circle railway line, up Grange Road, along Wingrove Street..." (Darebin Heritage, Mr Harold Harless) to the corner of Duncan and Gillies Streets.

The St Andrews Presbyterian Church Hall was not destroyed in the 1948 fire but was demolished and replaced after 1959 (Public Building File PB7021A, PROV) and is now the site of the St Andrew's Alphington and Fairfield Uniting Church Kindergarten.

The site of the original St Andrews Presbyterian Church was near the junction of the Outer Circle Railway and Heidelberg road (Heidelberg News and Greensborough and Diamond Creek Chronicle, 4 September 1915, p. 2). This location had been selected with the intention of being convenient to residents of both Alphington and Fairfield (Heidelberg News and Greensborough and Diamond Creek Chronicle, 4 September 1915, p.2) suggesting that the church was built after the first subdivision of the Fairfield Park Estate in 1883 (Fairfield Park 1883, State Library of Victoria). When the church committee concluded that the location was no longer suitable the present church site at the south east corner of Gillies and Duncan Streets Fairfield was purchased and the building was relocated to Fairfield on 26 September 1915 (Heidelberg News and Greensborough and Diamond Creek Chronicle, 2 October 1915, p. 2). The original intention was that a new Presbyterian church would be erected at Alphington for the convenience of parishioners located in that area however it is unclear whether or not that church was ever built or the land purchased (Heidelberg News and Greensborough and Diamond Creek Chronicle, 4 September 1915, p.2). On 21st October 1916 a new brick Methodist Church was dedicated and opened on land adjacent to the existing Methodist Church at Alphington (Spectator and Methodist Chronicle, 25 October 1916, p. 1388).

The relationship between the Methodist and Presbyterian congregations at Alphington and Fairfield is illustrated by their respective honour roles for soldiers who died serving in WW1 (Figure H4 and Figure H5). An honour roll was prepared for each congregation and they are both listed on the Victorian heritage Database (Victorian Heritage War Inventory, Hermes 190108 and 190109, Victorian Heritage Database) (Figure H4 and Figure H5). However, both honour rolls are stored at St Andrew's Alphington and Fairfield Uniting Church Fairfield. The removal of the Alphington memorial to Fairfield may have occurred when the St Andrew's Alphington and Fairfield Uniting Church was formed in 1977 The Methodist church was sold and converted to a private residence.

The St Andrew's Alphington and Fairfield Uniting Church congregation have been part of the broader Fairfield community since around the time the suburb was created in the 1880s. As a pivotal spiritual and social component of the Fairfield community Church members have engaged in fundraising for

local charities, community projects and church projects since their inception. At various times they have operated a Sunday School, held events for children, organised educational and social services for members of the community of all ages and have been actively involved in raising funds and providing support to soldiers and their families during times of war.

### Architect F Bruce Kemp

Architect Francis Bruce (Bruce) Kemp is believed to have been born around 1897 (Embarkation Records WW1, Australian War Memorial). He studied architecture at technical college from 1915 to 1917 (Royal Victorian Institute of Architects (RVIA) Past Members Personal Files, State Library of Victoria (SLV) and following his graduation won a bronze medal in 1918 from the RVIA for measured drawings of the portico of the Melbourne Town Hall. (RVIA Journal, 1918, p. 7) In the same year Kemp enlisted in the AIF where he served for one year. (RVIA, Past Members Files, SLV) On his return from service he won first prize in a competition to design an honour roll for the Newtown Presbyterian Church. (Geelong Advertiser 20 October 1919 Trove)

Bruce Kemp was the nephew of prominent architect Henry Hardie Kemp with whom he articulated for four years (RVIA, Past Members Personal Files, SLV) before passing his examination in architecture at Melbourne in 1920 (RVIA, Past Members Personal Files, SLV). He was registered an associate of The Royal Victorian Institute of Architects (RVIA) in the same year and won first prize from the Kew Council in July of 1921 for the design of another war memorial although this design was never used. (Monuments Australia website). The following year he was awarded first place from the RVIA for "A Design for a City Building for Architects". (RVIA Minutes of Annual General Meeting, 1922, p. 14)

Following his registration with the RVIA Bruce Kemp undertook some commissions on his own (The Argus, 20 October 1920, p.4, Trove) before forming a partnership with his uncle that practiced as H H and F B Kemp of 30 Queen Street Melbourne. Murray Forster articulated with Bruce Kemp at this time. (Allen Walsh, Murray Forster Story, p.47) In 1925 The Sydney Morning Herald (SMH) reported that Bruce Kemp had returned from four years of architecture study overseas although no further details can be found about this trip and how it was managed within the architecture firm. (Sydney Morning Herald 15 December 1925, Trove) The partnership of F B Kemp with his uncle is described by Tibbits as having ended in 1929 when H H Kemp retired. (George Tibbits, Kemp, Henry Hardie, Australian Dictionary of Biography, 1983) however H H and F B Kemp continue to advertise for tenders for projects as late as 1938. H H Kemp died in 1946 (George Tibbits, Kemp, Henry Hardie, Australian Dictionary of Biography, 1983)

Projects known to have been undertaken by H H and F B Kemp include:

- Memorial Club Rooms at Portland Victoria (Construction & Local Government 29 September 1921, p. 18, Trove)
- Presbyterian Girls Hostel Gippsland Street North Melbourne (The Argus 30 November 1926 p. 14, Trove)
- Plans for the National War Memorial at Canberra for which they received a 100 guinea honorarium as one of the 12 most favoured designs (SMH 25 February 1927, p. 12, Trove)
- Presbyterian Manse Mentone (The Age, 11 January 1930, p. 1, Trove)



- Additions to the Beaconsfield Public Hall (The Age 23 July 1930, p. 5, Trove)
- Warehouse and Office Building on the site of former St Andrews Presbyterian Church Carlton, cnr. Queensbury and Drummond Streets (The Argus, 17 October 1938, p. 2 Trove)
- Additions to the Melbourne Eye and Ear Hospital (The Age 20 July 1928, p.10, Trove)
- Presbyterian Church Assembly Hall 156-160 Collins Street (The Age, 19 January 1935, p.26, Trove)
- Brick Factory at Carlton (The Age, 14 June 1938, p.11, Trove)
- Children's Aid Society of the Presbyterian Church of Victoria Elgar Road Burwood (The Age, 9 June 1936, p.17, Trove)

F Bruce Kemp, L Hume Sherrard and K Murray Forster worked as architects in conjunction for the Kildonan Children's Home building complex in Burwood, in 1936. The trio designed the administration building, two cottages and possibly the school building. (Miles Lewis in RVIA Journal 1936 cited by RBA Architects and Conservation Consultants Pty Ltd, 16 March 2015 for City of Whitehorse)

Following the Kildonan project Kemp and Forster worked together regularly as architects in conjunction for a number of years. Allen Walsh recounts that it was common when working together that Forster would do the design and specifications and Kemp would do the supervision. (Allen Walsh, Murray Forster Story, p.53)

Projects that Kemp and Forster worked on together included:

- Various projects at St Leonards Presbyterian Ladies College as the school architects from the 1930's to the 1950's (Allen Walsh, Murray Forster Story, p.47)
- Weatherboard Church Hall and Classroom Maribyrnong (The Argus 23 January 1937, p. 10, Trove)
- Converting a residence to flats at St Kilda Road Brighton, (The Argus 3 February 1937, p. 17, Trove)
- John Barnaby Kindergarten (The Argus 3 April 1937, p. 18, Trove)
- John Barnaby Kindergarten (The Argus 3 April 1937, p. 18, Trove)
- New Burwood Presbyterian Church (The Argus, 1 May 1937, p.20, Trove)
- Master Plan for Haileybury College Brighton Victoria (Construction 10 December 1947, p.6, Trove) including Memorial Chapel and Hall (The Argus, 6 December 1947, p. 44, Trove)
- Timber framed classrooms for the Presbyterian Church at 163 South Road Brighton (Construction 1 December 1954, p. 9 Trove)
- Assembly Hall Haileybury College Brighton Victoria (Cross-Section no. 42, April 1956 cited by RBA Architects and Conservation Consultants Pty Ltd, 16 March 2015 for City of Whitehorse)

Kemp appears to have continued practicing in conjunction with Murray Forster until around 1954 after which time he continued working alone.

Concurrent with his work with Forster, Kemp had also undertaken individual commissions such as the St Andrews Presbyterian Church at Fairfield in 1949/50.

Examples of Kemp's later individual designs include:

- New brick Church at Lascelles (The Age, 6 August 1949, p.31, Trove)
- Brick Veneer, plaster wall residence East Brighton (The Age, 14 August 1954, p.29, Trove)
- Sunday School Hall East Oakleigh Presbyterian Church (The Argus, 30 October 1954, Trove)
- The Patch', Patch Road Dandenongs (The Argus, 22 November 1954, p. 9, Trove)
- St Leonards Uniting Church Brighton, 1956 (Docomomo Australia website, 2014)
- Brick veneer residence North Brighton (The Argus, 14 April 1956, Trove)
- Brick cottage at Melbourne Orphanage Brighton (The Argus, 5 May 1956, p.24, Trove)

F Bruce Kemp's architecture is not well known or studied, however, the list of his works presented in this report indicates that he was a highly accomplished architect whose clients included churches and private schools, primarily those associated with the Presbyterian Church. Much of Kemp's work, in partnership or alone, was commissioned by or related to the Presbyterian Church. This relationship was not exclusive however and across his career of at least 35 years, Kemp worked on a range of other buildings for a variety of clients and used a diverse range of materials to meet the functional and aesthetic aspirations of each project.

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The Age, 14 June 1938, p.11, Trove

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The Age 10 November 1949, p. 2, Trove

The Age 13 September 1952, p.16, Trove

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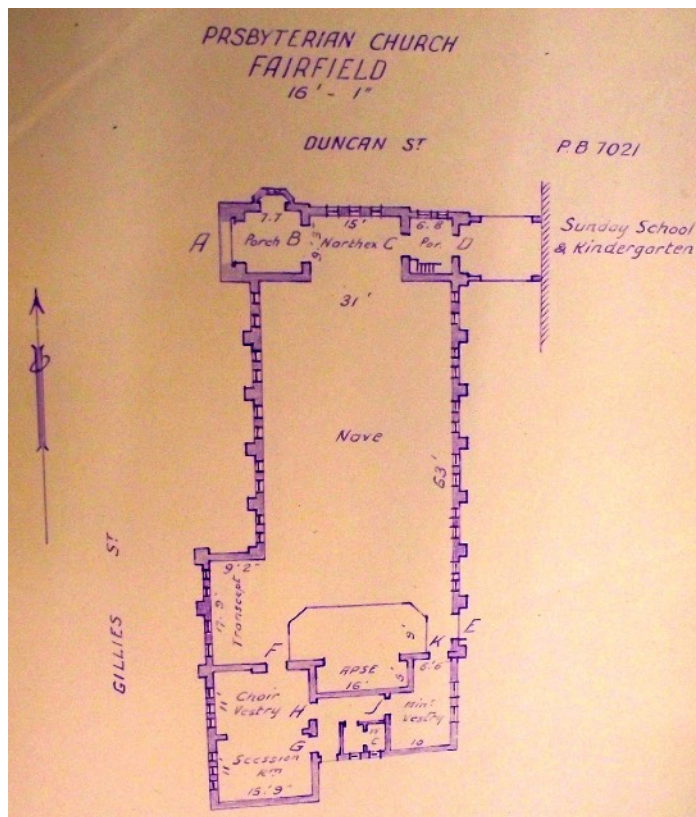
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## History - Illustrations



**Figure H1.**

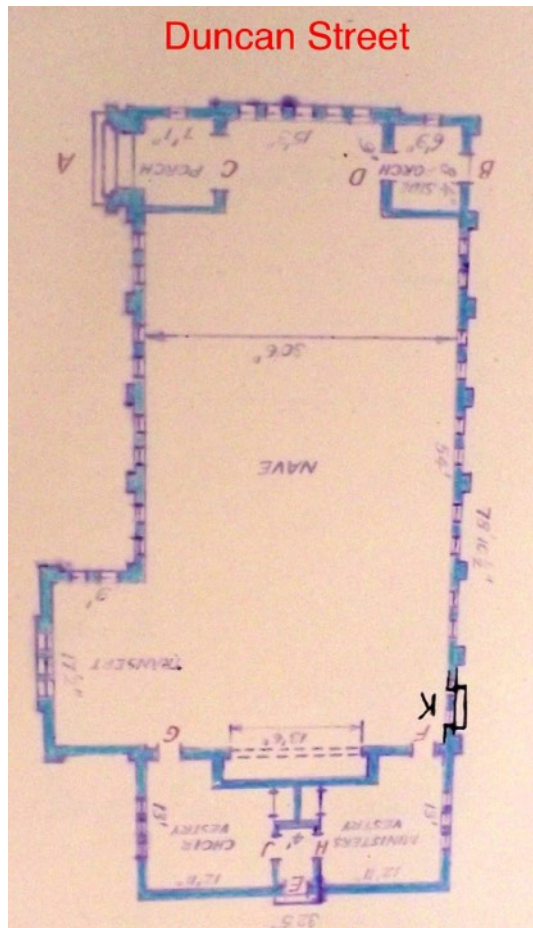
Showing the Right Reverend W D Marshall, Moderator of the Presbyterian Assembly laying the foundation stone (Figure H6) for the new St Andrews Alphington and Fairfield Presbyterian Church Fairfield on 9 December 1950, Source: The Argus 11 December 1950, page 4



**Figure H2**

Original approved plan submitted to Heidelberg Shire Council by F Bruce Kemp for St Andrews Alphington and Fairfield Presbyterian Church, Fairfield, 1949 (Public Building File 7021, Public Record Office Victoria (PROV))



**Figure H3**

Amended approved plan submitted to Heidelberg Shire Council by F Bruce Kemp for St Andrews Alphington and Fairfield Presbyterian Church, Fairfield, 1950 (Public Building File 7021, PROV)

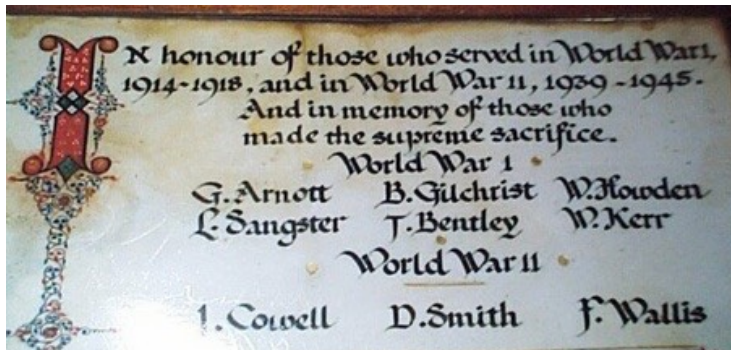
**Figure H4**

Image showing part of Fairfield St Andrew's Presbyterian Church Honour Roll (First World War) Victorian War Heritage Inventory Source: Victorian Heritage Database

**Figure H5**

Image showing part of Empire Honour Roll, Alphington Methodist Church (First World War) Victorian War Heritage Inventory  
Source: Victorian Heritage Database

**Figure H6**

Foundation Stone set into the north exterior wall facing Duncan St. It is polished granite with the words revealed by horizontal stripes of a finely coursed surface.

## Description

This section describes the place in 2017. Refer to the Place History for additional important details describing historical changes in the physical fabric.

The c1950 Post-War (Mid Twentieth Century) Gothic building is very intact. The landmark setting of the large imposing building with a picturesque arrangement of steeply pitched and tiled gable roof forms, of earthy colours and textures, is clearly designed to address both Gillies and Duncan Streets. The entry porch is off Gillies Street. Overall the design is asymmetric, although the three gable ends, including the whole of the north elevation facing Duncan St, are symmetrical in design. The north facade is minimalist and modernist in design, whereas the west elevation facing Gillies St has a deeply sculptured design more typical of Gothic churches. The Arts and Crafts materials of earthy coloured clinker bricks and marseille terra cotta tiles dominate and unify the whole building. (Figures D1-D6)

The interior has cream coloured face brickwork forming a modernist dado, whilst the walls above are rendered and painted.

The c1950 building is in very good condition with excellent integrity. All other structures on the site are not significant.

### Descriptions - Illustrations.

All photos taken in 2016-17 by Heritage Intelligence Pty Ltd

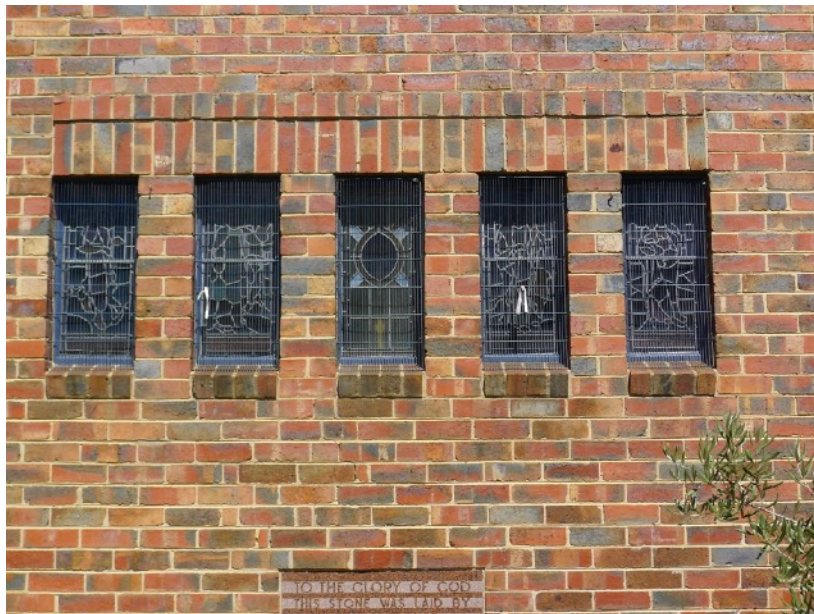


**Figure D1.** View showing the landmark setting of the large imposing building of earthy colours and textures, clearly designed to address both Gillies and Duncan Streets. The entry porch is off Gillies Street. The north facade facing Duncan Street is minimalist in design, whereas the west elevation facing Gillies St has a deeply sculptured design more typical of Gothic churches. The Arts and Crafts materials of earthy coloured clinker bricks and marseille tiles dominate and unify the whole building. The low flat topped timber picket fence is recent but appropriate.





**Figure D2.** Understated detailing using clinker bricks to create a finely articulated surface including two lines of brick (one headers and one stretchers) following the angle of the gable end, a thin corbelled projection, and a cross made entirely of bricks set vertically. Overall the north facade has a minimalist and modernist approach to the design.



**Figure D3** Understated detailing using soldier clinker bricks to create a 'lintel' over the five deeply set rectangular windows with clinker brick sills. The lead lights are a more traditional design. Note the position of the Foundation Stone below the windows.



**Figure D4.** The traditionalist Gothic design of the entry porch, facing Gillies St. The pointed arches are emphasised by the three rows of soldier headers over the entry porch, and three rows of stretchers following the line of the gable end. Vertical 'engaged piers' support the entry arch and an unpainted crest in-relief of the Presbyterian Church of Australia is visible, in the gable end.



**Figure D5.** The west elevation facing Gillies St has a deeply sculptured design with pointed arches, buttresses, which are more typical of traditionalist Gothic churches.





**Figure D6.** Clinker brick walls, buttresses, pointed arched windows and window sills. Narrow three light timber windows.

## Comparative analysis

The spiritual and social significance of churches such as St Andrew's Alphington and Fairfield Uniting Church is representative of the vital role these institutions have played and continue to play within their communities. Churches of all denominations are considered by many, to be central to spiritual health and the celebration of births, deaths, marriages and special events.


To date one of the three Churches with addresses in Fairfield (Figure C1) and 18 other Churches from the City of Darebin are subject to the City of Darebin Heritage Overlay. (Table 1) (Schedule to the City of Darebin Heritage Overlay). However, all of the churches pre 1960 are more traditionalist in design than the church by Kemp, and there are no other churches by the architect of St Andrew's Alphington and Fairfield Uniting Church.





**Figure C1** St Paul's Anglican Church,  
88A Station Street Fairfield c.1916  
City of Darebin HO190  
Architects Thomas Watt & Sons

Source: Heritage Intelligence Pty Ltd.



**Table 1** Churches from the City of Darebin subject to the City of Darebin Heritage Overlay (sourced from the Schedule to the Heritage Overlay, City of Darebin).

Name	Image	Status
Bulgarian Eastern Orthodox Church (Former Anglican Church of the Epiphany) 1-3 Bayview Street Northcote		Bulgarian Eastern Orthodox Church City of Darebin HO10  Source Victorian Heritage Database (VHD) webpage & Google Maps


<p>Former Primitive Methodist Church now Salvation army Hall 18 Mitchell Street Northcote</p>		<p>Former Primitive Methodist Church c. by 1889 City of Darebin HO145</p> <p>Source VHD webpage</p>
<p>Baptist Church 540-542 High Street Northcote</p>		<p>Baptist Church Northcote c.1909 City of Darebin HO192</p> <p>Source VHD webpage &amp; Google Maps</p>
<p>Former Presbyterian Church (&amp; Hall) 40-42 James Street Northcote</p>		<p>Presbyterian Church Northcote c.1894 City of Darebin HO53</p> <p>Architect J Preston</p> <p>Source VHD webpage, Google Maps &amp; Australia's Christian Heritage webpage</p>
<p>Sacred Heart Catholic Church (complex including Church, Rectory, Hall and School) 322 Bell Street Preston 4-6 Clifton Gove and 89 David Street Preston</p>		<p>Sacred Heart Catholic Church c.1926 City of Darebin HO199</p> <p>Source VHD webpage, Google Maps.</p>



<p>All Saints Anglican Church (complex) 400 High Street Preston &amp; 239 Murray Road Preston</p>		<p>All Saints Anglican Church, City of Darebin HO225 c.1930 (church) c.1956 (tower)</p> <p>Architect Louis Williams</p> <p>Source VHD webpage &amp; Google Maps</p>
<p>Holy Name Catholic Church (&amp; Primary School) 2-26 Robb Street Reservoir</p>		<p>Holy Name Catholic Church c.1939-1964 City of Darebin HO249</p> <p>Architect J P Saraty</p> <p>Source VDH webpage</p>
<p>St Cyril &amp; Methodius Greek Orthodox Church (Former South Preston Methodist Church) 2 Yann Street Preston</p>		<p>St Cyril &amp; Methodius Greek Orthodox Church c. 1888-1889 City of Darebin HO94</p> <p>Architect Alfred Dunn</p> <p>Source VHD webpage, Google Maps &amp; Darebin Heritage website</p>
<p>St Mark's Anglican Church (&amp; Vicarage) 19-21 Beatty Street Reservoir (relocated from Camberwell 1930)</p>		<p>St Mark's Anglican Church</p> <p>City of Darebin HO261</p> <p>Source VHD webpage</p>

<p>Former Methodist Church 34 George Street Reservoir</p>		<p>Former Methodist Church c. 1934 City of Darebin HO264</p> <p>Architect Raymond G Wilson</p> <p>Source VHD webpage</p>
<p>Uniting Church 648-656 High Street Reservoir</p>		<p>Uniting Church Reservoir c. 1863 additions c.1902</p> <p>City of Darebin HO49</p> <p>Source VHD webpage &amp; Google Maps</p>
<p>Regent Baptist Church (&amp; other buildings in complex) 726-734 High Street Reservoir</p>		<p>Regent Baptist Church c. 1964 City of Darebin HO271</p> <p>Architect Keith Reid</p> <p>Source VHD webpage</p>
<p>St George's Church of England (sic Anglican) 32-34 Ralph Street Reservoir</p>		<p>St George's Anglican Church c.1964</p> <p>City of Darebin HO279</p> <p>Architects Mockridge, Stahle and Mitchell</p> <p>Source VHD webpage &amp; Google Maps</p>







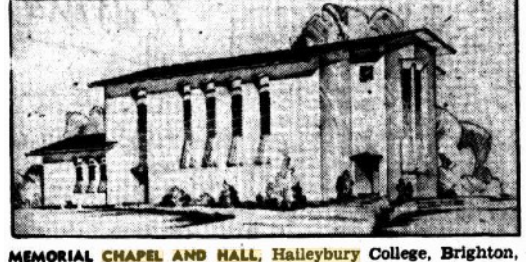
<p>St Mary's Church 718-730 High Street Thornbury</p> <p>(Building is actually at cnr. High Street &amp; Rossmoyne Street, address listed above and in schedule to heritage overlay appears to be incorrect)</p>		<p>St Mary's Catholic Church Thornbury</p> <p>City of Darebin HO46</p> <p>Image from Google Street View.</p>
<p>St Gabriel's Catholic Church (&amp; Presbytery) 237-243 Spring Street Reservoir</p>		<p>St Gabriel's Catholic Church c.1960</p> <p>City of Darebin HO280</p> <p>Architect S J Moran</p> <p>Source VHD webpage &amp; Google Maps</p>
<p>Thornbury Uniting Church 7-15 Rossmoyne Street Thornbury</p>		<p>Thornbury Uniting Church c.1924 City of Darebin HO291</p> <p>Architect Harry Norris</p> <p>Source VHD webpage &amp; Google Maps</p>
<p>Antiochian Orthodox Church of St George (Formerly Holy Trinity Anglican Church (&amp; Vicarage &amp; Parish Hall) 26-28 Shaftesbury Parade Thornbury</p>		<p>Antiochian Orthodox Church of St George City of Darebin HO151</p> <p>Architect Carleton &amp; Carleton</p> <p>Source VHD webpage &amp; Google Maps</p>

Uniting Church (Former Methodist) (complex) 797-809 Heidelberg Road Alphington		Uniting Church Alphington City of Darebin HO38  Source VHD webpage & Google Maps
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F B Kemp and the two architects he worked closest with, K M Forster and H H Kemp had in common a close relationship with the Presbyterian Church. Many commissions Kemp undertook in partnership or by himself were for premises associated with or owned and/or operated by the Presbyterian Church of Australia. These projects included St Leonards Presbyterian Ladies College and the Haileybury School, John Barnaby Kindergarten, Kildonan Children's Home, Presbyterian Girls Hostel, a warehouse and office building on former Presbyterian Church owned land and various church halls, school buildings and manses. Surviving churches identified as having been designed by F B Kemp (alone or in partnership) (Table 2) are typically in light coloured brick with a variety of roof cladding. St Andrew's Alphington and Fairfield Uniting Church has many architectural features which are also seen in the other known examples, however, they are combined in an entirely different manner and none are constructed in clinker brick. (The Argus 1 May 1937, p. 20, The Argus 5 March 1938, p. 32, and The Argus, 16 July 1938, p. 6, Trove.

**Table 2.**

Name	Image	Status
<p><b>1937</b></p> <p>Uniting Church            Burwood corner of            Warrigal Road and            Hyssop Street            Camberwell</p> <p>Confirmation of            address located in            Camberwell, from The            Age 10 May 1937,            Trove</p>	<p><b>New Burwood Presbyterian Church</b></p> 	<p>Plan for "Burwood            Presbyterian Church"  <b>1937</b>            Architects  <b>Kemp and Forster</b></p> <p>Source: The Argus 1 May            1937, p. 20, Trove</p>

<p><b>1937</b></p> <p>Uniting Church Burwood corner of Warrigal Road and Hyssop Street Camberwell</p>		<p>Burwood Uniting Church at Camberwell Kemp and Forster, originally completed with 'provision' for tower and additions which may not have been finished (The Age 10th May 1937 Trove)</p> <p>Source: Google Maps</p>
<p><b>1938</b></p> <p>Uniting Church Glen Huntly corner of Grange Street and El Nido Grove Glen Huntly</p>		<p>Plan for Presbyterian Church at Glen Huntly Architects <b>Kemp and Forster. 1938</b></p> <p>Source: The Argus 5 March 1938, p. 32, Trove</p>
<p><b>1938</b></p> <p>Uniting Church Glen Huntly corner of Grange Street and El Nido Grove Glen Huntly</p>		<p>Uniting Church at Glen Huntly Street View March 2013, the church roof tiles have since been replaced with grey colourbond</p> <p>Source: Google Maps</p>
<p><b>1947</b></p> <p>Chapel and Hall Haileybury College Brighton</p>		<p>Sketch of planned memorial chapel and hall at Haileybury College Brighton 1947. Architects <b>F B Kemp and R M Forster</b> Source: The Argus 6 December 1947, p. 44, Trove</p>



<p><b>c1956</b></p> <p>St Leonards Uniting Church Cnr New Street and Wolseley Grove Brighton</p>		<p>St Leonards Uniting Church c.1956</p> <p><b>Bruce Kemp as sole architect</b></p> <p>Source: Docomomo Australia website, St Leonards Uniting Church. Image. Google Street View.</p>
<p><b>1938</b></p> <p>Dreeite South Presbyterian Church</p>	<p><b>NEW CHURCH AT DREEITE SOUTH</b></p> 	<p>Sketch for Presbyterian Church Dreeite South. 1938</p> <p>Architects <b>HH and FB Kemp</b></p> <p>Source: The Argus, 16 July 1938, p. 6, Trove</p>
<p><b>1938</b></p> <p>Former St Andrew's Uniting Church 545 South Dreeite Road Alvie</p>		<p>Former St Andrew's Uniting Church Alvie c. 2015 Colac Otway Shire HO187</p> <p>Source: L J Hooker Real Estate website</p>
<p>Unsure of location, church may not survive or may not have been built</p>	<p>Brick Church Lascelles no image available, identified from newspaper article calling for tenders in The Age, 6 August 1949, p.31, Trove</p>	<p>Uniting church Lascelles F B Kemp &amp; R M Forester 1949 - 1954</p>

St David's Anglican Church Moorabbin (Figure C2), designed by well known architect Louis R Williams, is especially comparable, as it is similar style, materials and time period as St Andrew's Alphington and Fairfield Uniting Church. However, search of the VHD has been conducted for the terms post-war, gothic, churches, old English and English gothic. Only a single church was identified in the VHD that has gothic features and was designed post war (1962), and it is also by architect Louis R Williams, however it is of a different style to the St Andrew's Alphington and Fairfield Uniting Church. (Figure C3)



**Figure C 2**

St David's Anglican Church Moorabbin c.1948  
City of Kingston

Architect Louis R Williams

Source. Building and Engineering Journal March 24, 1948, p 25.

Photo. Heritage Intelligence Pty Ltd 4 April 2017



**Figure C 3** St Andrew's Anglican Church, Brighton.  
c.1962

Architect Louis R Williams

Part of the St Andrew's Church Precinct. Brighton, listed on the Victorian Heritage Register (VHR HO124) and the Bayside City Schedule to the Heritage Overlay (HO96)

Source VHD webpage

## References

Building and Engineering Journal March 24, 1948, p 25.

Cox Geoffrey 2012 in Organ Historical Trust Australia, 'St Mary's Catholic Church cnr High Street and Rossmoyne Street, Thornbury', viewed 22 April 2017, <<http://www.ohta.org.au/organs/organs/ThornburyStMarys.html>>

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Google Maps, viewed 22 April 2017, <<https://maps.google.com.au/>>

Google Maps Photograph, Jim Dale 5 April 2009, St David's Anglican Church Moorabbin, viewed 22 April 2017, <<https://www.google.com.au/maps/uv?hl=en&pb=!1s0x6ad66eb1fbf13e1b:0xc0ee96e0615da21b!2m19!2m2!1i80!2i80!3m1!2i20!16m13!1b1!2m2!1m1!1e1!2m2!1m1!1e3!2m2!1m1!1e5!2m2!1m1!1e4!3m1!7e115!4shttps://ssl.panoramio.com/photo/25235181>>



5sst+dauids+moorabbin+church+-+Google+Search&imagekey=!1e4!

2s25235181&sa=X&ved=0ahUKEwi3oJOhLjTAhUGjpQKHckgBDsQoioIYTAK>

L J Hooker Real Estate, 'Former St Andrews Uniting Church Alvie', viewed 22/4/2017, <<https://www.realestate.com.au/sold/property-villa-vic-alvie-120529189>>

Schedule to the City of Darebin Heritage Overlay, viewed 22 April 2017, <[http://planning-schemes.delwp.vic.gov.au/schemes/darebin/ordinance/43\\_01s\\_dare.pdf](http://planning-schemes.delwp.vic.gov.au/schemes/darebin/ordinance/43_01s_dare.pdf)>

Victorian Heritage Database, viewed 22 April 2017 <<http://vhd.heritagecouncil.vic.gov.au/>>

## Management Guidelines

Whilst landowners are not obliged to undertake restoration works, these guidelines provide recommendations to facilitate the retention and enhancement of the culturally significant place, its fabric and its setting, when restoration works or alterations to the building are proposed. They also identify issues particular to the place and provide further detailed advice where relevant.

That c 1950 church is significant. The map below has a red rectangle which marks the approximate location of the property boundary and recommended HO boundary. The polygon shaded in blue illustrates the preferred location for additions and new development.

- Additions and new buildings
  - Retain clear views from the public realm, of the Gillies and Duncan street elevations.
  - The blue shaded area in the Map below is the preferred location for additions and new development.
- Reconstruction and Restoration
  - Never sand, water or soda blast the building as this will permanently damage the surface of the bricks, mortar and tiles
  - Do not paint or seal the brick work.
- Signage
  - Ensure all signage is designed to fit around the significant architectural design features, not over them.



## 4.2. 5 - 5A Railway Place, Fairfield

**Locality:** FAIRFIELD

**Place address:** 5- 5A RAILWAY PLACE

**Citation date** 2017

**Place type (when built):** Shops and residence

**Recommended heritage protection:** Local government level: No  
Vic Heritage Register: No  
Heritage Inventory (Archaeological): No  
Local Planning Scheme: No

**Place name:** Railway Place Therapy and Consulting Suites.



**Architectural Style:** Inter-War Art Deco

**Designer / Architect:** [Not Known]

**Construction Date/ builder:** c 1920/21 : George McCrohan (?), contractor of Alphington.

## Statement of Significance

This statement of significance is based on the history, description and comparative analysis in this citation. The Criteria, A, B, C, D, E, F, G, H is the Heritage Council Criteria for assessing cultural heritage significance (HERCON). Heritage values of historic, social, aesthetic and scientific significance are based on the Burra Charter definitions. Level of Significance, Local, State, National, is in accordance with the level of Government legislation.

### *What is significant?*

The premises at 5-5A Railway Place, Fairfield, constructed c1920, is of interest. The remnant original form, materials and details of the building, as constructed in c1920 are significant. Later buildings and alterations are not significant.

### *How is it significant?*

The premises at 5-5A Railway Place, Fairfield, is of local interest for its historical values.

### *What is significant?*

The premises at 5-5A Railway Place, Fairfield is **historically significant at a local level** for its continuous associations from its construction c1920, for over 97 years, with the earliest commercial development of Fairfield Village. 5A was first used for trading in small goods, but from 1925 it was leased by twelve different tenants for use as a confectionary shop for the following 49 years until at least 1974. 5 was used variously for trading in small goods, real estate agent, hairdressers & billiard saloon, fruiterer, printer and boot repairer. The businesses that operated from 5-5A Railway Place during the early to mid 20th century were typical of a suburban shopping strip servicing the everyday needs of nearby residents. (HERCON criterion A)

The premises at 5-5A Railway Place, Fairfield is **aesthetically of interest** for its unobstructed views to and from the railway station, and intact representative Inter-War Art Deco style architecture which presents primarily as a single storey set of two shops with a cantilevered verandah and a gate to a pedestrian walkway to the private residence at the rear. The Inter-War Art Deco style building has distinctive features which provide an articulated facade including rendered and stepped parapet with stylised low relief contrasting red brick 'cornice' with dentils below. The parapet is divided into three bays by rendered pilasters, with the central bay surmounted by a raised parapet. The ground floor shop fronts have been altered albeit sympathetically. Overall 5-5A does not meet the threshold for an individual Heritage Overlay.

(HERCON criterion E).

## Statutory Recommendations

This place is not recommended for inclusion in the Schedule to the Heritage Overlay of the City of Darebin Planning Scheme with an individual Heritage Overlay, with the boundaries as shown on the map.

<b>External Paint Controls</b>	No
<b>Internal Alteration Controls</b>	No
<b>Tree Controls</b>	No
<b>Outbuildings or fences which are not exempt under Clause 43.01-3</b>	No
<b>Prohibited Uses May Be Permitted</b>	No
<b>Incorporated Plan</b>	No
<b>Aboriginal Heritage Place</b>	Not assessed

## History

### Thematic context

This place is associated with the following themes identified in the City of Darebin Heritage Study Volume 1 Draft Thematic Environmental History Stage 2 Report (2008)

#### 4 Developing Darebin's Economies

##### 4.6 Providing local shopping and services

#### 5 Building Suburban Darebin

##### 5.3 Developing the suburban ideal and Twentieth century recovery

### Place history

Although originally styled Railway Place the present street was also referred to as Railway Place South to distinguish it from Railway Place North located opposite and across the railway line. In 1930 the name of Railway Place North was changed to Wingrove Street and although the name Railway Place South persisted in the vernacular and the geographical distinction was still present in the Street Directory (Melbourne Sands & McDougall Street Directory 1930-1974), the property was not thereafter rated as Railway Place South. For the purpose of this historical research the Street is referred to as Railway Place however where reference is made to the street prior to 1930 it may have been known as Railway Place South.

5/5a Railway Place Fairfield is part of land described as portion 113 Parish of Jika Jika, County Bourke that was granted to Frances Vidal 14 March 1840 (Application for Title 3482). Portion 113 appears to have remained unoccupied, and undeveloped until it came into the possession of Charles Henry James and Percy Dobson in 1883. James and Dobson owned the adjacent portion 114 and undertook multiple subdivisions of both properties between 1883 and 1885 in order to create the Fairfield Park Estate. (Certificate of Title Volume 1515, Folio 976) (Figure H1).

In 1883 Margaret Little purchased three allotments in the Fairfield Park Estate including portion 108 from which 5 and 5a Railway Place devolved. (Certificate of Title Volume 1531 Folio 006) In 1885 Margaret Little sold her property to Thomas Knight Bennett who transferred the land to his wife Amelia in 1887. (Certificate of Title Volume 1671 Folio 334114) The land remained undeveloped during the depression of the late nineteenth century but as the economy was recovering Amelia Bennett sold a small section of portion 108 that fronted Railway Place to the Victorian Railways and subdivided the remaining land into four allotments in 1911. (Certificate of Title Volume 1531 Folio 006) (Figure 2) Amelia sold the first of these allotments in 1914 and the remainder were disposed of by the executors of her estate between 1918 and 1922. (Certificate of Title Volume 1671, Folio 334114) (Figure H2)

The land which became 5 and 5a Railway Place was sold in 1919 to Oscar Langbridge Jackson from whom it was rapidly conveyed to Emma McCrohan and then her husband contractor George McCrohan of Alphington. (Certificate of Title Volume 4276 Folio 855134) George McCrohan is likely responsible for the construction of the two shops and home that were erected on the property in 1920/21. (Heidelberg Shire Rate Records, 1920-1921) Archibald McDonald Jnr purchased the property from McCrohan in 1921 and leased the premises to a succession of tenants until his death in 1956. (Certificate of Title Folio 4276 Folio 855134)

When first leased in 1922 5 and 5a Railway Place were occupied by a single tenant trading in small goods (Melbourne Sands & McDougall Street Directory 1923, Heidelberg Shire Rate Records, 1923-1924) and the following year three tenants occupied each part of the property. Although the layout of 5 and 5a Railway Place (Figure H3) enables the building to be occupied concurrently by one, two or three tenants, for thirty years from 1924 to 1954 (and potentially longer) the property was leased as a dwelling and shop at 5 Railway Place and a shop at 5a Railway Place. (Heidelberg Shire Rate Records 1924 - 1936 and Melbourne Sands Directory 1924-1974)

In 1925 a confectionery shop opened at 5 Railway Place and for the following 49 years until at least 1974 twelve different tenants operated a confectionery business from the premises. (Table 1) During this period 5a Railway Place was variously occupied by a milliner, real estate agent, tailor, hairdresser, fruiterer and printer before the tenancy was secured by a boot repairer in 1935. A boot repair shop was operated from 5a Railway place by two successive tenants for 29 years until at least 1974. (Table 1) The image below (Figure H4) likely dates from between 1928 and after 1946 (based on butchers sign at 9 Railway Place) when 5 and 5a Railway Place were operated as a confectionery and boot repair shop respectively.

**Table 1**

List of occupants by name and trade of 5 and 5a Railway Place from 1922-1973/1974 derived from the Heidelberg Shire Rate Records 1921-1936 and the Melbourne Sands & McDougall Street Directories 1921-1974.

Year	5 Railway Place		5a Railway Place	
	Name	Business	Name	Business
1922	Wallace	small goods	Gothard	small goods
1923	Bellett	small goods	Wallace	small goods
	Jenkins	knitted goods		
1924	Glanfield	house/confectioner	Torbey	real estate agent



1925			Bell & Poposch	hairdressers
1926			Jackson	hairdresser & billiard saloon
1927			Davey	hairdresser & billiard saloon
1928			Crompton	fruiterer
1929 - 1931			Wallace	printer
1932 - 1934	Jones	house/confectioner	Marshall	boot repairer
1935 - 1938			McIntyre	boot repairer
1939 - 1950	Harris	house/confectioner		
1951 - 1954	Walsh	house/confectioner		
1955	Chapman	house/confectioner	no separate listing in Sands Directory and Rate Records from this time have not been consulted	
1956 - 1957	Baker	house/confectioner		
1958 - 1959	Waller	house/confectioner		
1960 - 1966	Kenney	house/confectioner		
1967 - 1968	Earl	house/confectioner		
1969	Johnson	house/confectioner		
1970	Rush	house/confectioner		
1971 - 1973	Cowman	house/confectioner		

The businesses that operated from Railway Place during the early to mid 20th century were typical of a suburban shopping strip servicing the everyday needs of nearby residents. Other business' regularly trading in the street included the Returned Serviceman's Hall from 1921, billiard saloon from 1923 , bootmaker, dress maker, tobacconist, hairdresser, baker, pastry cook, chemist, fruiterer, grocer, costumer, butcher, real estate agent, newsagent, milliner and children's draper. (Melbourne Sands & McDougall Directory 1909 - 1974)

Since it ceased to trade as a confectionery sometime after 1974, businesses operating from 5 Railway Place have included Vox Bandicoot a children's theatre and educational services (AuzBiz.net website 2017) (Figure H5), The Phoenix Centre for Training and Services (The Australian Business Directory website 2017) and Perinatal Psychology (The Centre for Perinatal Psychology website 2017). The premises is currently home to the Railway Place Therapy and Consulting Suites. (Yellow Pages online directory 2017).

### Sources

Application for Title 3482, General Law Library

The Australian Business Directory website, viewed 16/4/2017. <[http://www.aubizs.com/business/Phoenix-Centre-For-Training\\_2jgK.html](http://www.aubizs.com/business/Phoenix-Centre-For-Training_2jgK.html)>

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Centre for Perinatal Psychology website, viewed 16/4/2017. <<http://www.centreforperinatalpsychology.com.au/location/fairfield/>>

Certificate of Title Volume 1515, Folio 976

Certificate of Title Volume 1531 Folio 006

Certificate of Title Volume 1671 Folio 334114

Certificate of Title Volume 4276 Folio 855134

Heidelberg Shire Rate Records, Fairfield Riding 1920-1921, VPRS2870/P0000/142, Public Record Office of Victoria (PROV)

Heidelberg Shire Rate Records, Fairfield Riding 1921-1922, VPRS2870/P0000/143, PROV

Heidelberg Shire Rate Records, Fairfield Riding 1922-1923, VPRS2870/P0000/144, PROV

Heidelberg Shire Rate Books, Fairfield Riding 1923-1924, VPRS2870/P0000/145, PROV

Heidelberg Shire Rate Records, Fairfield Riding 1924-1925, VPRS2870/P0000/146, PROV

Heidelberg Shire Rate Records, Fairfield Riding 1925-1926, VPRS2870/P0000/147, PROV

Heidelberg Shire Rate Books, Fairfield Riding 1926-1927, VPRS2870/P0000/148, PROV

Heidelberg Shire Rate Records, Fairfield Riding 1927-1928, VPRS2870/P0000/149, PROV

Heidelberg Shire Rate Books, Fairfield Riding 1928-1929, VPRS2870/P0000/150, PROV

Heidelberg Shire Rate Books, Fairfield Riding 1929-1930, VPRS2870/P0000/151, PROV

Heidelberg Shire Rate Books, Fairfield Riding 1930-1931, VPRS2870/P0000/152, PROV

Heidelberg Shire Rate Books, Fairfield Riding 1931-1932, VPRS2870/P0000/153, PROV

Heidelberg Shire Rate Books, Fairfield Riding 1932-1933, VPRS2870/P0000/154, PROV

Heidelberg Shire Rate Books, Fairfield Riding 1933-1934, VPRS2870/P0000/155, PROV

Heidelberg Shire Rate Books, Fairfield Riding 1934-1935, VPRS2870/P0000/156, PROV

Heidelberg Shire rate Books, Fairfield Riding 1935-1936. VPRS2870/P0000/157, PROV

Melbourne Sands & McDougall Directory 1907 to 1974

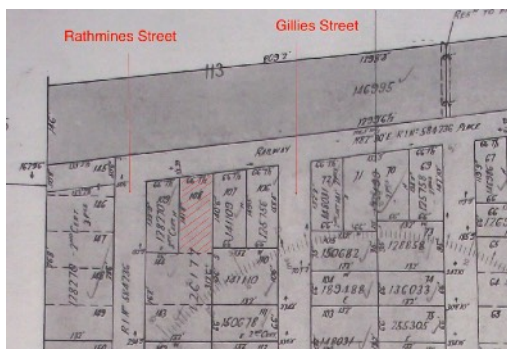
Plan of subdivision 534/755, Certificate of Title Volume 1515 Folio 976

Public Record Office Victoria, Railway Negatives, VPRS12800/P0003/44

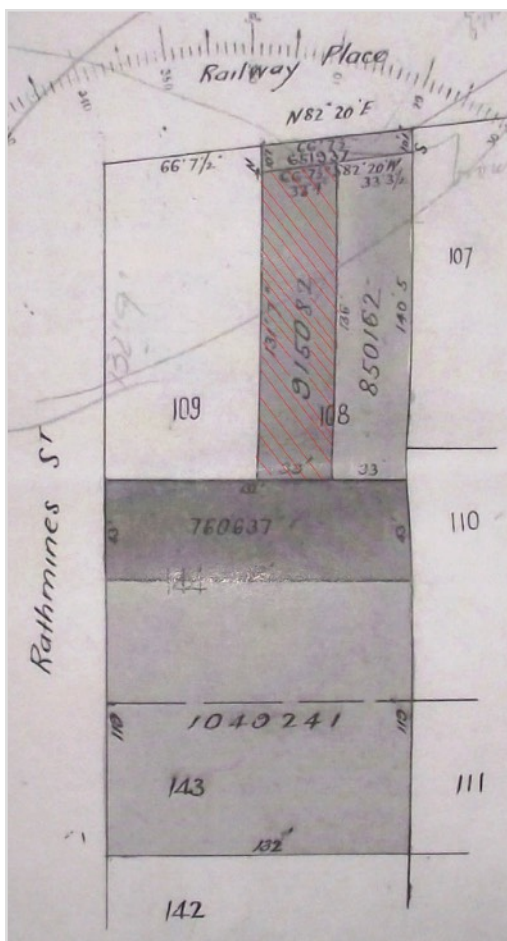
RealEstateView.com.au website, 2004 viewed 16/4/2017 <<https://www.realestateview.com.au/real-estate/5-railway-place-fairfield-vic/property-details-sold-residential-460509/>>

Yellow Pages online directory, viewed 16/4/2017. <<https://www.yellowpages.com.au/vic/fairfield/railway-place-therapy-consulting-12919303-listing.html>>

## History - Illustrations



**Figure H1.** Plan of subdivision showing area of 5 and 5a Railway Place South highlighted (Plan of subdivision 534/755, Certificate of Title Volume 1515 Folio 976)



**Figure H2** Subdivision plan dated 15 June 1911 of portion 108, the land later described as 5 and 5a Railway Place is highlight. (Certificate of Title Volume 1671, Folio 334114)



**Figure H3** Showing floor plan of 5 and 5a Railway Place c. 2004. Location of each shop and optional dividing wall highlighted. Source [RealEstateView.com.au](http://RealEstateView.com.au) website 2017.



**Figure H4** Railway Place showing the street front of 5 and 5a looking from Rathmines Street towards Gillies Street (estimated c. 1928-greater than c.1946). Photographer unknown, n.d., Source: Railway Negatives Public Record Office Victoria VPRS12800/P0003/44 (reproduced with permission).



**Figure H5** Photograph of 5 and 5a Railway Place when occupied by Vox Bandicoot, photographer unknown, c.2004. Source: Real Estate View Website 2017.

## Description

This section describes the place in 2017. ( Refer also to Appendix 8.8 for additional information.)

The c1920 place is a representative Inter-War Art Deco style building which presents primarily as a single storey building with two shop fronts, with a cantilevered verandah and a gate to a pedestrian walkway to the private residence behind the shops. The Inter-War Art Deco style building has distinctive features which provide an articulated facade including rendered and stepped parapet with stylised low relief contrasting red brick 'cornice' with dentils below. The parapet is divided into three bays by rendered pilasters, with the central bay surmounted by a raised parapet. The ground floor shop fronts have been altered, albeit sympathetically. ( Figure H5). The c1920 building is in very good condition, although there are some alterations to the shop fronts including the removal of the faience green tiles from the stall boards, and the insertion of new doors (Compare Figure D1 with Figure H5). The residence at the rear, is not visible from the street and it is not significant.

### Description - Illustrations.

All photos taken in 2016-17 by Heritage Intelligence Pty Ltd unless otherwise noted.



**Figure D1.** View showing the Inter-War Art Deco style features of rendered and stepped parapet with stylised low relief contrasting red brick 'cornice' with dentils below. The parapet is divided into three bays by rendered pilasters, with the central bay surmounted by a raised parapet. The ground floor shop fronts have been altered. Note the gate on the east side, which provided access to the residence at the rear. Source: Google street view.

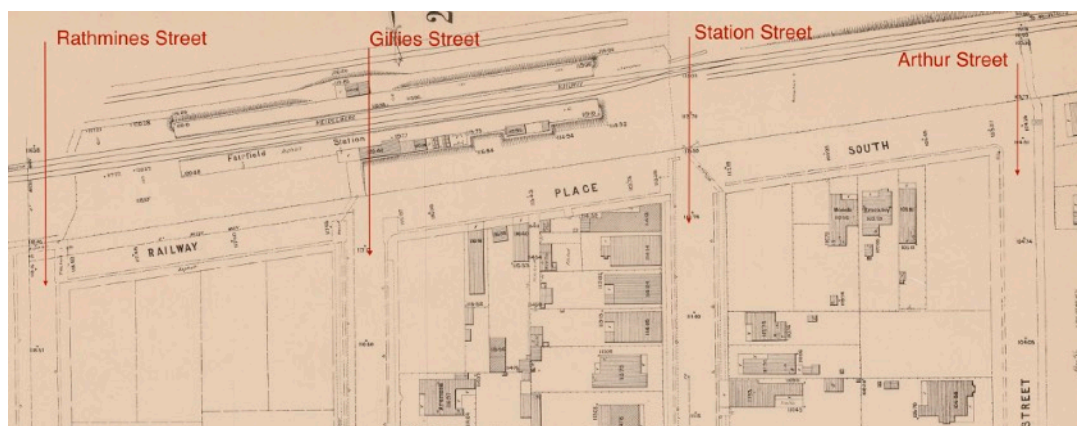


**FigureD2.** Detail of Figure H4. Railway Place showing the street front of 5 and 5a looking from Rathmines Street towards Gillies Street (estimated c.1928-greater than c.1946). Photographer unknown, n.d., Source: Railway Negatives Public Record Office Victoria VPRS12800/P0003/44 (reproduced with permission)



## Comparative analysis

5 and 5a Railway Place was constructed in 1920/1921 in response to a growing need for the provision of goods and services to an expanding Fairfield population driven by the interwar boom. The interwar boom came on the heels of the early twentieth century economic recovery from the late nineteenth century depression. During this depression and the early stages of recovery all of the block in Railway Place, from Rathmines Street to Gillies Street, was vacant land and by 1912 only four buildings had been erected on both blocks of Railway Place, between Gillies and Station Streets, and the three erected between Station and Arthur Streets appear to have been residences. (Figure C1) Of particular relevance to the growth of Fairfield was the development of local job providing industries taking advantage of an improved economic outlook and the arrival of electric rail services in 1921.



**Figure C1** Melbourne Metropolitan Board of Works Plan 2474, c.1912

Source State Library of Victoria

The construction of a commercial, rather than exclusively residential premises at 5 and 5a Railway Place was directly related to the proximity of the land to the Railway Station.

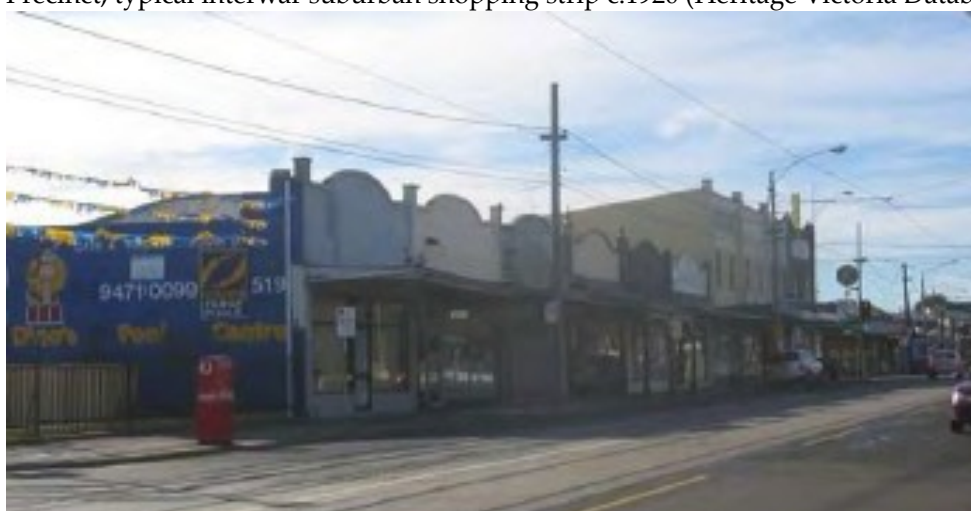
"Although there was a significant increase in car ownership during the inter-war period, many households did not have access to a car until after the Second War World. As a result, many services had to be provided within walking distance of households and every neighbourhood had its own local shopping centre providing essential items." (Context, City of Darebin Heritage Study, 2008, p.58)

Locating commercial premises proximate to the Railway Station enabled businesses to offer a convenient location and attract the custom of residents as well as passing trade.

Evolving in a similar fashion to Railway Place Fairfield and heavily affected by the electrification of the railway, a small shopping strip servicing the everyday needs of Reservoir residents developed on High Street (between Mason and Edgar Streets). (Context, City of Darebin Heritage Study, Historic Heritage Places, 2011, p. 88) (Figure C2) A similar commercial precinct developed at Plenty Road Preston in response to the improved services of the electric tramway in 1920. (Context, City of Darebin Heritage Study, Historic Heritage Places, 2011, p.528) (Figure C3)



**Figure C2..** 658-94 High Street (between Mason and Edgar Streets), part of Reservoir Commercial Precinct, typical interwar suburban shopping strip c.1920 (Heritage Victoria Database HO307)



**Figure C3.** 519-541 Plenty Road Preston, Darebin City, typical interwar suburban shopping strip of 10 businesses, c. 1926-1930 (Victorian Heritage Database HO241).

Both these precincts which are recognised on the Heritage Victoria Database are characterised by modest single storey interwar buildings similar to the building at 5 and 5a Railway Place Fairfield.

5 & 5a Railway Place is one of several significant shop and residence buildings in Fairfield examples of which are outlined in the table below (Table 1). These buildings are significant for their intactness, association with the commercial development of the suburb of Fairfield, as evidence of the fluctuating economic fortunes which directed that development and as a representation of the types of business and services used daily by suburban residents in the interwar and post war era. There are presently no listings of these types of typical suburban shopping strip buildings on the VHD for Fairfield. Whilst there are individual listings for some shops in the City of Darebin they are overwhelmingly two storey buildings from the Victorian to interwar period and do not reflect the more modest suburban village development that characterised the growth of Fairfield.

**Table 1**



Modest Single Storey Interwar shops and shops/residences identified as buildings of local significance to the community of Fairfield (Heritage Intelligence, Fairfield Village Heritage Assessment Progress Report Stage One, 2017)

Address	Image	Construction Date and Purpose
11 Railway Place Fairfield		<p>Shop built c.1919 occupied by Youren Estate Agents 1919-1923 then for a short time a furniture warehouse then a grocers business from 1925 until around 1941 after which time it was vacant for several years</p> <p>Source Google Street View. Melbourne Sands &amp; McDougall 1919-1974 Melbourne Metropolitan Board of Works (MMBW) Plan 2474 c.1912</p>
17-17a Railway Place Fairfield		<p>Originally Shop &amp; House  built c. 1912 - 1921/22</p> <p>Source Google Street View. Heidelberg Shire Rate Records 1921/1922 MMBW Plan 2474 c.1912</p>
21-23 Railway Place Fairfield		<p>Originally Shop &amp; Large Shop  built c.1912 - 1921/22</p> <p>Source Google Street View. Heidelberg Shire Rate Records 1921/1922 MMBW Plan 2474 c.1912</p>
25 Railway Place Fairfield		<p>Originally Shop &amp; House  Possibly built pre 1912</p> <p>Source: Google Street View. MMBW Plan 2474 c.1912</p>



76 Station Street Fairfield		Originally site occupied by residence name 'Edgeware' built pre 1912 Shop built after 1912  Source: Heritage Intelligence P/L MMBW Plan 2473 c.1912
78 Station Street Fairfield		Originally site occupied by residence name 'Edgeware' built pre 1912  Shop built after 1912  Source: Heritage Intelligence P/L MMBW Plan 2473 c.1912
80 and 82 Station Street Fairfield		Originally site occupied by residence name 'Olivet' built pre 1912  Shop built after 1912  Source: Heritage Intelligence P/L MMBW Plan 2473 c.1912
84 Station Street Fairfield		Originally part of St Paul's Church of England land  Built after 1912  Source: Heritage Intelligence P/L MMBW Plan 2473 c.1912
120 Station Street Fairfield		Built after 1912  Source: Heritage Intelligence P/L MMBW Plan 2473 c.1912



125 Station Street Fairfield		Built after 1912  Source: Heritage Intelligence P/L MMBW Plan 2473 c.1912
282 Wingrove Street Fairfield  (previously 10 Railway Place North)		Shop and House Built c. 1920/21 first tenant a contractor, then music teacher and a confectioner from 1926 to at least 1946 Source: Heritage Intelligence P/L MMBW Plan 2473 c.1912 Sands & McDougall 1921

## References

Context, 2011, City of Darebin Heritage Study, Historic Heritage Places

Context, 2008, City of Darebin Heritage Study Volume 1 Draft Thematic Environmental History, Stage 2 Report, August 2008

State Library of Victoria, Melbourne Metropolitan Board of Works Plan 2473, c.1912, viewed 18 April 2017 <[http://digital.slv.vic.gov.au/view/action/singleViewer.do?dvs=1492610598882~998&locale=en\\_US&metadata\\_object\\_ratio=10&show\\_metadata=true&VIEWER\\_URL=/view/action/singleViewer.do?&preferred\\_usage\\_type=VIEW\\_MAIN&DELIVERY\\_RULE\\_ID=10&frameId=1&usePid1=true&usePid2=true](http://digital.slv.vic.gov.au/view/action/singleViewer.do?dvs=1492610598882~998&locale=en_US&metadata_object_ratio=10&show_metadata=true&VIEWER_URL=/view/action/singleViewer.do?&preferred_usage_type=VIEW_MAIN&DELIVERY_RULE_ID=10&frameId=1&usePid1=true&usePid2=true)>

State Library of Victoria, Melbourne Metropolitan Board of Works Plan 2475, c.1912, viewed 18 April 2017 <[http://digital.slv.vic.gov.au/view/action/singleViewer.do?dvs=1492610534586~672&locale=en\\_US&metadata\\_object\\_ratio=10&show\\_metadata=true&VIEWER\\_URL=/view/action/singleViewer.do?&preferred\\_usage\\_type=VIEW\\_MAIN&DELIVERY\\_RULE\\_ID=10&frameId=1&usePid1=true&usePid2=true](http://digital.slv.vic.gov.au/view/action/singleViewer.do?dvs=1492610534586~672&locale=en_US&metadata_object_ratio=10&show_metadata=true&VIEWER_URL=/view/action/singleViewer.do?&preferred_usage_type=VIEW_MAIN&DELIVERY_RULE_ID=10&frameId=1&usePid1=true&usePid2=true)>

State Library of Victoria, Melbourne Sands & McDougall 1909 - 1946

Heritage Victoria Database HO307, viewed 18 April 2017, viewed 18 April 2017, <[hd.heritagecouncil.vic.gov.au/places/27319](http://hd.heritagecouncil.vic.gov.au/places/27319)>

Victorian Heritage Database, Plenty Road Preston, HO241, viewed 18 April 2017, <[hd.heritagecouncil.vic.gov.au/places/27129](http://hd.heritagecouncil.vic.gov.au/places/27129)>

Heritage Intelligence Pty Ltd for City of Darebin, 20 March 2017, Fairfield Village Heritage Assessment Progress Report Stage One, 2017

## Management Guidelines

Whilst landowners are not obliged to undertake restoration works, these guidelines provide recommendations to facilitate the retention and enhancement of the culturally significant place, its fabric and its setting, when restoration works or alterations to the building are proposed. They also identify issues particular to the place and provide further detailed advice where relevant.

That part of the property that is visible from Railway Place, which is primarily the c1920 shops and elevation, is of interest. The red rectangle marks the approximate location of the property boundary and Heritage Overlay which is not recommended for this property. The blue polygon illustrates the preferred location for additions and new development.

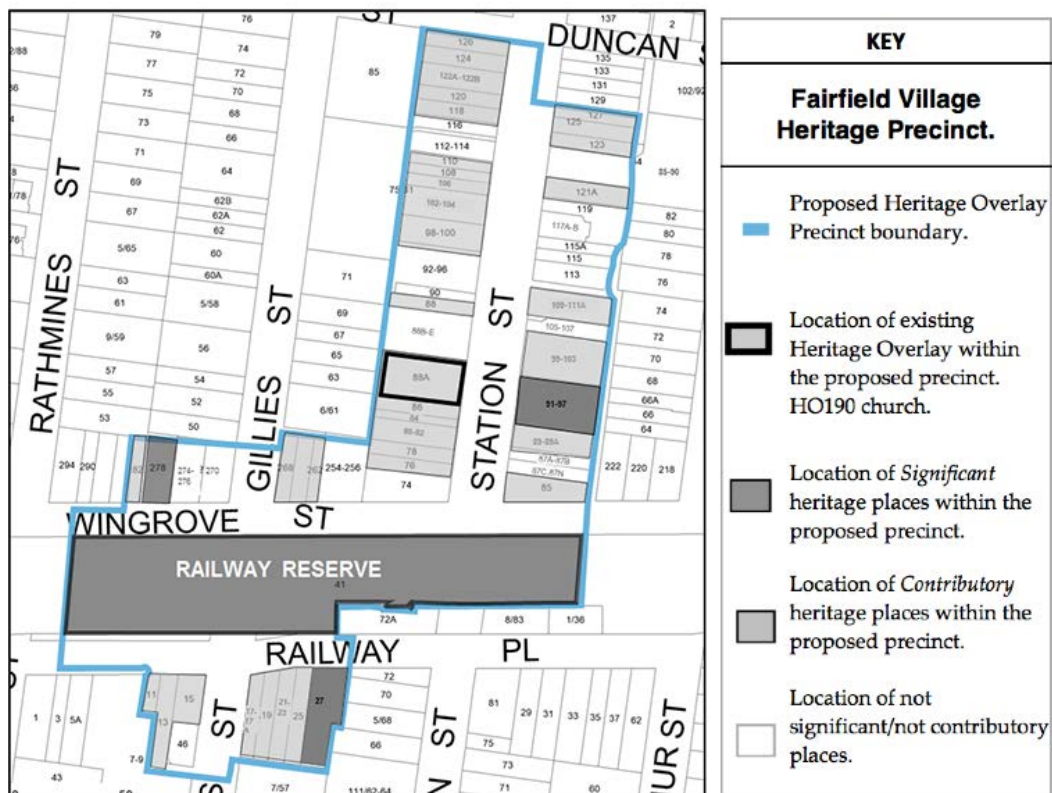
- Additions and new buildings
  - Retain clear views of the front elevation.
  - Retain the visual connection to and from the Railway Station and railway reserve.
  - The blue shaded area in the map below is the preferred location for additions and new development.
- Reconstruction and Restoration
  - Never sand, water or soda blast the building as this will permanently damage the bricks, mortar and render.
  - Restore the front elevation with shop fronts to their c1920 design.
- Signage
  - Ensure all signage is designed to fit around the significant architectural design features, not over them.



## 5. Citation - Precinct

### 5.1. Fairfield Village Heritage Precinct

Locality:	FAIRFIELD
Place address:	RAILWAY PLACE, RAILWAY RESERVE, STATION STREET, WINGROVE STREET, as shown in the precinct map below.
Citation date	2017
Place type (when built):	Primarily commercial places.
Recommended heritage protection:	Local government level: Yes Vic Heritage Register: No Heritage Inventory (Archaeological): No Local Planning Scheme: Yes
Architectural Style:	Predominantly Federation era and Inter-War era styles



**Figure 5.1.** Map and Key of the Fairfield Village Heritage Precinct. A larger image is in Appendix 8.2.

## Statement of Significance

This statement of significance is based on the history, description and comparative analysis in this citation. The Criteria, A, B, C, D, E, F, G, H is the Heritage Council Criteria for assessing cultural heritage significance (HERCON). Heritage values of historic, social, aesthetic and scientific significance are based on the Burra Charter definitions. Level of Significance, Local, State, National, is in accordance with the level of Government legislation.

### *What is significant?*

The Fairfield Village Heritage Precinct, comprising the Federation and Inter-war commercial buildings, the church and the railway reserve and station buildings, platforms, footbridge and signal box, and its spacious setting in the precinct as shown on the precinct map, is significant. The original form, materials and details of the heritage places as shown in the schedule, are significant as are views to and from the station. Other buildings and non-original alterations are not significant.

### *How is it significant?*

The Fairfield Village Heritage Precinct is locally significant for its historical, social and aesthetic values.

### *What is significant?*

**Historical Significance. (HERCON criteria A, B, H).**

#### **Railway Station as the Catalyst for the Earliest Development**

The Fairfield Village Heritage Precinct, **is historically significant at a local level** as it demonstrates the evolution of the modest working man's commercial shopping area from the earliest location of Fairfield's shopping village, located in Railway Place in the late 1880s and opposite the south side of the railway station, which was the catalyst for the development of Fairfield in the 1880s. The existing building on the north-east corner of Wingrove and Gillies Streets, and the existing church in Station Street, also date from the earliest period of development of Fairfield. The two distinctive palms in the Railway Reserve, flanking the entry to the southern entry to the station, are the only remnants of the Federation era garden plantings at the station.

#### **Impact of the Electrification of the Railway**

The shopping village later extended to the north side of the station buildings (Wingrove Street) and, after the electrification of the railway line in 1921, the extension of the shopping village continued around the corner into Station Street northwards to Duncan Street. The large numbers of inter-war style buildings in Railway Place, Wingrove and Station Streets reflect the significance the electrification of the line, to the economy of Fairfield. The former 1925 Lodge Medical practice at 278 Wingrove Street was strategically located opposite the station.

#### **Historic Village 'Square'**

The open space of the railway reserve together with the heritage features including but not limited to the station buildings, signal box, pedestrian bridge and palm trees within it. They illustrate the Railway Reserve's historic role, acting as the 'traditional village square' providing an informal community meeting place. Significance includes the setting, the functional inter relationship of the various parts, as well as views within and across the Railway Reserve from Wingrove Street and Railway Place which are vital to understand and enjoy the important heritage values. The spacious and visually connected context of the historic places within the railway reserve and to the historic buildings in Railway Reserve, Wingrove St and Station Street is in danger of being cumulatively eroded through the redevelopment of strips of railway reserve land for the construction of buildings which block the historic views. (Refer to Figure 5.2 for a diagram of the village 'square'.)



## Over 100 years as a Railway Station

The existing Federation era station building complex, including the station buildings, platforms, footbridge and signal box, is historically significant for indirectly recalling its opening in 1888 and remaining in use for its original purpose at present (over 106 years). It is important also for its capacity to offer an insight into how pre-electronic railway stations worked, and the important work of the chief architect's office following its establishment in 1908 when the Railways Department was engaged in the improvement of station facilities across the network in response to unprecedented traffic growth. This is illustrated by the Arts and Crafts style of the station buildings enabling it to be readily identified with the early work of the chief architect's office.

## Social Significance (HERCON criterion G)

The Fairfield Village Heritage Precinct, is **socially significant at a local level** particularly the railway reserve which is a spacious landmark in the precinct and seen as a focus and an extension of the local community performing functions similar to that of a civic space, where social and economic exchanges take place, where people and cultures mix as they use the railway infrastructure and as an historic and spacious back drop to shopping and eating in the streets visually connected to it. It is the only spacious landmark in the Fairfield Village Heritage Precinct.

## Aesthetic Significance (HERCON criterion E)

### Views.

The Fairfield Village Heritage Precinct, is **aesthetically significant at a local level** for the informal 'village square' created by the unobstructed views to and from the Federation and Inter-war historic places in and around the Railway Reserve. The 'village square' is lined with buildings, infrastructure, community facilities and open spaces, particularly in the railway reserve. It is a self-identified precinct which is seen as a focus and an extension of the local community and in this way performs functions similar to that of a civic space, like town squares in old European cities. The low rise one or two storey structures, the articulation of the building facades, the Federation and Inter-war decorative elements, varying pediments and broken skyline, provide an interesting and diverse streetscape that is divided into narrow allotments and buildings which have a human scale. The two distinctive palms flank the entry to the front of the 1911 station buildings and this landmark statement, which can be seen from various parts of the precinct, is important. (Refer to Figure 5.2 for a diagram of the village 'square'.

## Arts and Crafts Architecture

The station buildings are aesthetically significant as an increasingly rare, substantially complete and interrelated railway complex of the Federation era, with two palms, and the picturesque Arts and Crafts forms, motifs including strap work, spandrels, fibrous cement slates and rough-cast plaster work.

## Inter-war Architectural Streetscapes

Station Street is aesthetically significant for the streetscapes of predominantly Inter-war historic commercial buildings of low rise one or two storey structure, the articulation of the building facades, the moderately Inter-war decorative elements, varying pediments and broken skyline, they provide an interesting and diverse streetscape that is divided into narrow allotments and buildings which have a human scale.

## Statutory Recommendations

This place is recommended for inclusion in the Schedule to the Heritage Overlay of the City of Darebin Planning Scheme as part of the Fairfield Village Heritage Precinct, with the boundaries as shown on the map.

<b>External Paint Controls</b>	Yes
<b>Internal Alteration Controls</b>	No
<b>Tree Controls</b>	Yes. Two palm trees on the railway reserve only.
<b>Outbuildings or fences which are not exempt under Clause 43.01-3</b>	No
<b>Prohibited Uses May Be Permitted</b>	No
<b>Incorporated Plan</b>	Yes. Darebin HO Permit Exemptions 2011 amended 2017
<b>Aboriginal Heritage Place</b>	Not assessed

### Other Recommendations

It is recommended that the Fairfield Village heritage precinct is added to the Heritage Overlay of the Darebin Planning Scheme. The extent of registration is the whole of the precinct as defined by the precinct map.

## History

### Thematic context

This place is associated with the following themes identified in the City of Darebin Heritage Study Volume 1 Draft Thematic Environmental History Stage 2 Report (2008)

- Peopling Fairfield
- Transport and Communications
- Developing Fairfield's Economy

### Peopling Fairfield

Although the land that comprises present day Fairfield was initially sold in the 1840's, the buyers were chiefly investors who didn't reside on their property. Whilst nearby suburbs progressed at a steady pace, the suburb of Fairfield took a while to get going. The apparent disinterest of speculators in their investments was possibly the onset of economic depression which dogged much of the 1840s. Some investors sold early but most realised little return during this time, apparently not even succeeding in acquiring tenants.

C H James, an entrepreneur and property speculator, created the Fairfield Park Estate from F Vidal's Crown Portion 114 (early in the 1880s). The Fairfield park subdivision was sold in generous suburban allotments at auction in November 1883 and then by successive 'extensions' to the subdivision from 1884 to at least November 1885 when extension 26 was sold (Library of Victoria, Fairfield Park no.26). The sales and the hype around the early auctions in particular, included in some cases a champagne luncheon held for the benefit of purchasers (Figure H1) (The Lorgnette, 25 September 1883, p. 2). This extravagance was typical of the property boom that preceded a crash in property prices and an economic depression in the late nineteenth century (Context, 2008, p. 145 ). A railway had

run through the estate from Clifton Hill to Alphington since 1883, but did not connect directly with Melbourne. The promise existed to extend this line to the then rural Heidelberg, which was realised in 1888 but ended in failure. To augment this directionless line, James had constructed a horse tramway from the Fairfield Park Station (later known as Fairfield Station), up Station Street for over a mile, which also ended in failure when it closed in 1890.

## Developing Fairfield's Economy

Growth in Fairfield in the Victorian era was further stunted by the collapse of the economy in 1891 and the resulting great depression.

'All over Melbourne land companies were floated to buy land for quick resale at large profits, resulting in fortunes being made and lost. As land prices spiralled, banks and building societies over-reached their lending capacities and borrowers borrowed beyond their capacity to repay. Eventually, in December 1891 the whole structure began to collapse. People lost their savings, building stopped and housing estates remained vacant. The collapse resulted in a disastrous depression that brought unemployment and misery to many thousands, and halted further development for a decade or more.' (Context, 2008, pp 28-29)

As the economy recovered across the turn of the century (Context, 2012, pp 155 & 11), the improved railway services of the Heidelberg line which was opened in 1901 reduced travel time from Fairfield to Melbourne rendering Fairfield a more attractive residential option. Popular suburbs including Collingwood and Fitzroy were overcrowded and the population of Fairfield grew in response to 2,500 (Darebin Heritage, Fairfield).

Repeated incidents of flooding continued to inhibit growth in the Fairfield until the low areas were drained in 1907/1908 (Victorian Places, Fairfield).

Railway Place reflects the slow pace of the development of Fairfield pre WW1. Around 1912 there were only 5 properties directly fronting Railway Place in the precinct (Figure H2). Three of these between Gillies and Station Streets lie within the precinct boundaries (Figure H4).

In 1921, the rail line was electrified and an article in the *Weekly Times* referred to Station Street and Railway Place Fairfield as "fast becoming busy thoroughfares...(with) many shops of the modern type." in contrast to the Fairfield of "not so many years ago which was almost a wilderness with unmade streets, a few houses here and there, and half a dozen shops. Although only five miles from the city, it was like living in the country." (Weekly Times, 1921, page 48)

The growth boom in Railway Place Fairfield post WW1 resulted from;

- draining of the lower parts of Fairfield making it more attractive for residential occupation thereby increasing customers (Darebin Heritage, Fairfield)
- making of Railway Place north and south (The Age, 10 February 1913, p.16)
- conversion to electrical rail service (1921) (Weekly Times, 25 June 1921, page 48, Trove)
- increased demand and overcrowding in suburbs more proximate to the city (Darebin Heritage, Fairfield)
- increased opportunities for local employment with the Australian Paper Mill using the old outer circle rail line, accessed from the junction at Fairfield Station, as a branch line (1919) (Darebin Heritage, Amcor, The Australian Paper Manufacturing Company) and growth at the Queens Memorial Hospital at the old Yarra Bend Asylum site (1917 and 1923) (Darebin Heritage, Fairfield Infectious Diseases Hospital)

The electrification of the network in the early 1920s played an important role in the development boom that swept much of Darebin during the inter-war period.

Being immediately adjacent to the Fairfield Railway Station, the development of Railway Place followed the fortunes of the available transport options and related population expansion.

Railway Place remained central to community transport needs, and was therefore exposed to commercial traffic despite the inadequate nineteenth century train services. Options developed to supplement the limitations of the train service included buses (Context, 2008, pp 38 & 39) and "...a gas engine fitted to a carriage which operated briefly..." (Context, 2008, p. 35) until the superior service of the Heidelberg line opened in 1901 (Context, 2008, p. 36).

The importance of Railway Place as a thoroughfare meant that a pedestrian bridge was constructed to enable foot traffic across the railway line in March 1914 (Heidelberg News and Greensborough and Diamond Creek Chronicle, 14 March 1914, p. 2). (Figure H4)

The c 1912 Melbourne Metropolitan Board of Works detail plan 2473, (Figure H5) shows that there were only 11 properties (7 residential, 1 commercial, 1 church, 1 town hall and 1 unknown) directly fronting Station Street in the precinct. The development of the suburb in the late nineteenth and early twentieth century was illustrated by the building of community facilities such as the first St Paul's Anglican Church in Station Street, erected in 1885, which also served as the district's first school.

In 1921 the Weekly Times credited the Fairfield community for their "mutual help and generous and active support (for) local affairs...(explaining) somewhat the rapid progress that is being made there." (Weekly Times, 25 June 1921, p. 48)

## Transport and Communication

Transport and the lack of it, is a pivotal theme in the development of Fairfield. The c1883 Plan of original subdivision of Fairfield Park Estate (Source SLV) shows the Fairfield Park station as a feature at the top of the map. ( Figure H8).

The present station buildings were erected by the Lang Brothers under a contract signed on 26th. April, 1911. An earlier signal box had been opened on 23rd. March, 1891 to coincide with the opening of the Outer Circle line, but this was replaced with the existing one in 1913. The station name was changed to Fairfield on 14th. November, 1943. The level crossing gates at Station Street and associated mechanism in the signal box were removed and replaced with automatic boom barriers on 2nd. November, 1969. The signal box remaining open for train movements in connection with the paper mill siding and for special workings after that time. It was finally closed on Monday, 28th. February, 2000 following the disconnection of the Australian Paper Manufacturers' siding. It is historically important as the earliest surviving signal box of its type in the metropolitan area and the equal oldest of its type in the State, comparing in this respect with the Creswick signal box.

The existing station building complex is historically significant as the passenger and goods station for its locality since construction in 1911, indirectly recalling its opening in 1888 and remaining in use for its original purpose at present. It is important also for its capacity to offer an insight into the important work of the chief architect's office following its establishment in 1908 when the Railways Department was engaged in the improvement of station facilities across the network in response to unprecedented traffic growth. Whilst this design compares closely with Montague (demolished) and loosely with nearby Westgarth, it appears not to be one of a number of similar stations, the adoption of the Arts and Crafts mode, however, enabling it to be readily identified with the early work of the chief architect's office.

It is aesthetically important as an increasingly rare, substantially complete railway complex of the Federation era, the Arts and Crafts motifs including strap work, spandrels, fibro cement slates and rough cast work being hall marks of J. W. Hardy's office at the time. Situated in between Railway Place to the south, and Wingrove St to the north and at the southern entry to the Station Street shopping centre, the complex, complete with signal box, imparts character to the centre and has become a local icon, valued by the community.

During the inter-war period the Heidelberg line was electrified (1921) and the large numbers of inter-war style buildings in Railway Place and Station Streets reflect the significance this event to the economy of Fairfield. Thus the electrification of the railway, reinforces the historic, aesthetic and social links of the interwar commercial buildings with the railway station.



The electrified railways sped up the journey to the city considerably and ran at more frequent intervals thus encouraging a new wave of suburban development (City of Darebin Heritage Study, Volume 1, Draft Thematic Environmental History, Stage 2 Report, August 2008, page 36). In response to the increase in population in the Darebin area, “ new buildings were erected during the first decades of the twentieth century at the busiest stations including Fairfield, Merri, Northcote, Thornbury and Bell. ” (City of Darebin Heritage Study, Volume 1, Draft Thematic Environmental History, Stage 2 Report, August 2008, page 36). The growth in Railway Place reflects this change, the street fronts being almost completely filled with commercial premises between Rathmines and Station streets (Figure H9), constructed in the inter war architectural styles. (Victorian Heritage Database, Fairfield Railway Station HO106).

In 1894 the Victorian Railways sought to encourage railway station staff to beautify their stations by instituting a prize for the most beautiful metropolitan and regional railway station (Mercury and Weekly Courier, 8 March 1894, p. 3 ). Before the prize was announced it appears that Fairfield Railway Station was already known for its attractive garden ornamentation (Mercury and Weekly Courier, 13 September 1894, p. 2) and in 1894 and 1895 they achieved second place and in 1896 won the competition (The Age, 12 June 1896, p. 4).

Two distinctive palms located at the front of the station facing Railway Place are typical garden features of the Federation era. The palms may therefore date from the same period of construction as the existing Federation era station buildings. It is also possible that the palms are the remains of plantings that formed part of the nineteenth century beautification program or that they date from around 1938 when some garden improvements are known to have been made at the Fairfield Railway Station (The Age, 6 October 1938, p.4). Whichever time they were planted the palms are the only remaining plantings from the early period of the Fairfield Railway Station which is now dominated by more recent plantings of gum trees. The street art ‘Fido’ is a more recent ‘beautification’ action in the railway reserve. The larger than life sculpture is a landmark feature and an example of what can be a positive contribution to the railway reserve area that enhances rather than detracts from the visual connection between the shopping area and the historic station.

Railway Place Fairfield evolved into a commercial centre based on the proximity of the location to the railway line and the requirement for a small commercial centre to serve a burgeoning population with limited resources for travel. The design of the strip reflects the trends of the inter war era, which time it experienced peak growth and the minor modifications since, reflect the continued development of the community of Fairfield.

The character of the Wingrove and Station Streets Commercial precinct as a thriving commercial centre has developed as a result of the initial size and orientation of the subdivision, limited initial development leaving vacant land ready for commercial occupation and the proximity of the streets to the Fairfield Park Railway Station and other public transport options. The contemporary reliance on cars and convenient car access to Fairfield combined with an active community has enabled the precinct to become a shopping destination.

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**Note:** A history of street name changes.

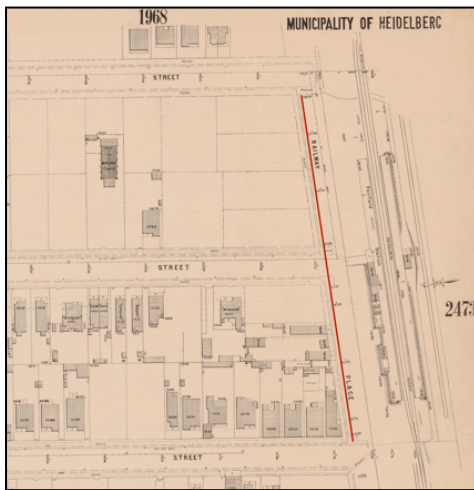
Wingrove Street originally extended to the east from Arthur Street alongside the railway line. During this time Wingrove Street between Arthur Street and Rathmines Street was known as Railway Place North. Likewise for a period of time Railway Place was known as Railway Place South. Sometime since 1945 changes were made to the road alignment at the termination of Arthur Street. The modification connected Wingrove Street and Railway Place together as a single thoroughfare (currently connected by a roundabout) known as Wingrove Street and the names Railway Place North and Railway Place South ceased to be used.

## History - Illustrations



**Figure H1.**

Part of advertisement illustrating the 'suburban land sale of Mr C H James' Fairfield Park'. Source. The Weekly Times n.d. (Darebin Heritage, Fairfield).



**Figure H2**

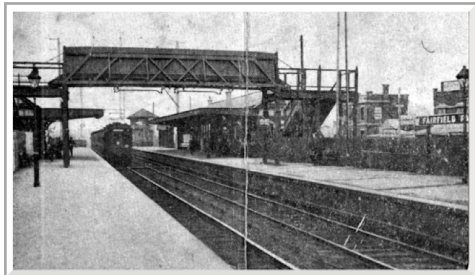
Plan c.1912 showing eastern frontage to Railway Place in the current Commercial 1 zoned land, highlighted by red line.

Source: Melbourne Metropolitan Board of Works Plan 2474, c 1912.



**Figure H3**

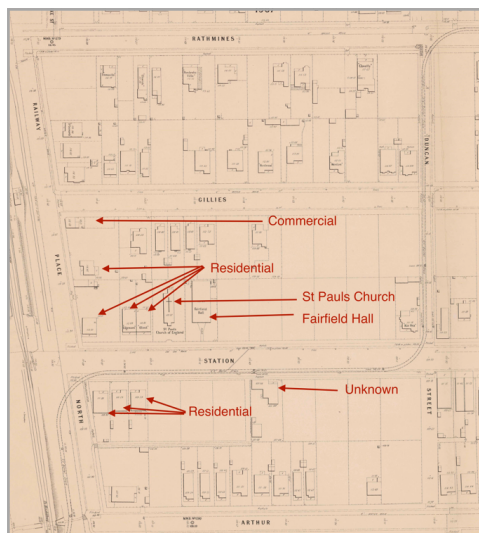
Showing buildings in Railway Place commercial area between Station Street and Gillies Street including the State Savings Bank (nearest to camera) and two adjoining buildings which may have been erected prior to 1912. Photographer unknown c. 1910-1930, Source State Library of Victoria



**Figure H4**

Showing view from Fairfield Park railway station platform showing pedestrian bridge linking Railway Place to Wingrove Street. Note signal box in the distance, shops and SSBank on Railway Place. photographer unknown n.d. Source (Darebin Heritage, Fairfield Railway Station).





**Figure H5**

Plan c.1912 showing developments in Station Street and Railway Place North (now Wingrove St) in the current Commercial 1 zoned land.

Source: Melbourne Metropolitan Board of Works Plan 2474, c 1912



**Figure H6** View of Station Street, looking north from railway line c. 1920s, showing rapid growth since the 1912 MMBW plan above, note Bell Accor building on the right with first floor arched window openings. (Images of Yesteryear, Fairfield). Image supplied by Images of Yesteryear. Contact Images of Yesteryear for reproduction in any form.



**Figure H7** c.1960s View of Station Street Fairfield looking south towards railway line, note the landmark quality of the large junction signal box to the right, in the distance and the absence of the street trees seen in the c1920 photo (Fig 3.6). Access to Station Street by car was made more appealing by its convenient access to the main arterial Heidelberg Road. Source Public Record Office Victoria, VPRS12800/P0001/H3176



**Figure H8** Plan showing the Fairfield Park Station and a steam train (detail). Source: State Library of Victoria.



**Figure H9** Railway Place looking east from Rathmines Street towards Gillies Street and Station St. The signal box (1913) and station buildings (1911) are visible behind the railway fence. The State Savings Bank is visible in the distance fronting Railway Place, and several shops. Photographer unknown, n.d., source Railway Negatives Public Record Office Victoria VPRS12800/P0003/44



## Description

This section describes the place in 2017. Refer to the Place History for additional important details describing historical changes in the physical fabric.

The historic streetscapes, include numerous unobstructed views to and from the railway station and Railway Place, Wingrove Street and the northern corner of Station Street, including the landmark statement by two distinctive palms, which are the only remaining plantings from the early period of the station, flanking the entry from Railway Place to the front of the 1911 station buildings. (Figure 5.2) The increasingly rare, substantially complete and interrelated railway complex of the Federation era, with picturesque Arts and Crafts forms, motifs including strap work, spandrels, fibrous cement slates and rough cast work being hall marks of J. W. Hardy's office at the time.

The streetscapes are predominantly Inter-war historic commercial buildings of low rise one or two storey structures, with articulated building facades, moderately Inter-war decorative elements, varying pediments and broken skylines, which provide an interesting and diverse streetscape that is broken up into narrow allotments and buildings which have a human scale.

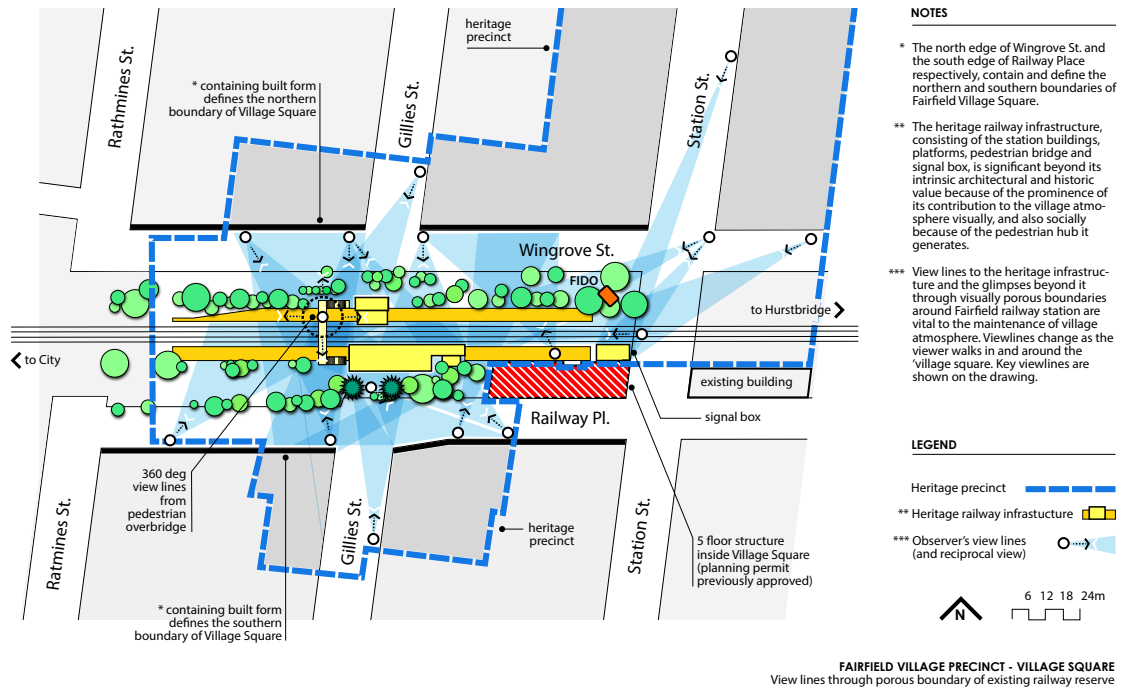
It is noted that recent developments outside the proposed heritage precinct are significantly larger by comparison to the historic places, particularly the floor area and heights, and they are wider in street frontage, and therefore they dominate the views to and within the commercial zone, over the smaller more human scale places. In some parts of the Commercial 1 zoned land, the large developments have reduced the strength of the visual connectedness of heritage places close to them. For example, as expert evidence to the 72A Railway Place VCAT hearing - (Dinopoulos v Darebin CC [2017] VCAT 118' VCAT P1228/2016) heritage expert Michael Taylor, acknowledged that the signal box within HO106 will be obscured from view from the south, particularly from the section of the village south of the railway line in Station St and Railway Place, but justified it by saying it will still be able to be viewed from the north. This line of argument could potentially be used to see one view at a time cut off by the next ad hoc development along railway land. It is for this reason that the boundary of the proposed heritage precinct does not include any of the large developments already built or approved. Refer to Figure 5.2 for a diagram of the 'village square' and views within it.

There are 90 places in the precinct, of which 60 are heritage places. ( These figures are approximate as it depends on whether they are counted according to the allotments shown in the cadastre map, or by the addresses including subdivision allotments.

Many shops retain significant decorative elements such as lead light windows, copper, brass or other metal window frames, pressed metal verandah or ingo soffits, glazed tiles on the shop piers and or the stall boards below the windows, sculptured rendered parapets with horizontal string courses, vertical pilasters, broken pediments of various designs, many formed by a raised and stylised 'capital' sometimes above a pilaster. There are also sculptures in relief stating the name of the historic building.

A detailed citation for 278 Wingrove St is included in Appendix 8.7.

The 'village' is lined with buildings, infrastructure, community facilities and open spaces, particularly in the railway reserve. It is a self-identified precinct which is seen as a focus and an extension of the local community and in this way performs functions similar to that of a civic space, like town squares in old European cities.



**Figure 5.2 (also at Figure 3.12)** Detail diagram of the 'Village Square', Fairfield Village Heritage Precinct developed and documented in Stage Two.

## Descriptions - Illustrations

All photos taken in 2016-17 by Heritage Intelligence Pty Ltd



**Figure D1.** View illustrating one of the larger and very wide two storey heritage places divided into narrow shop fronts which is reflected in the design of the first floor rendered walls. The first floor is divided into vertical bays with pilasters emphasised by a large round arched window between them, the high parapet is divided into three bays which is emphasised by the broken skyline. Thin horizontal lines balance the verticality of the narrow bays.



**Figure D2.** Distinctive parapets of two historic places illustrate the variations in design of the broken skyline and the more human scale achieved by the division of these large wide buildings into narrower bays.



**Figure D3.** Typical example of a modest single storey unpainted red brick historic place with a cantilevered verandah, and intact unpainted rendered parapet with broken skyline, formed by pilasters with simple 'capitals' and the name of the original building complex in relief, '1935 BUILDINGS' from the Inter-war era of development. It is likely that this was built as a set of three.



**Figure D4.** The streetscape view illustrates the variety in heights of the one and two storey heritage places and skylines using similar materials and features on the buildings: rendered parapet using pilasters to divide them into narrow bays as well as pilasters (in brick and/or render) at each end of each property.



**Figure D5.** The butcher shop illustrates an Inter-war shop front with a large window and highlight windows above it, surrounded by a thin metal window frames and glazed tiles on the piers and deep stall board below the window frame. (The door is more recent.)





**Figure D6.** The top lights of this shop window are in the Art Deco style and the ceiling of the ingo is pressed metal.



**Figure D7.** Several shop front windows have retained their Inter-war thin copper/brass metal window frames, deep stall boards of glazed tiles, ingoes with encaustic tiled floor patterns.

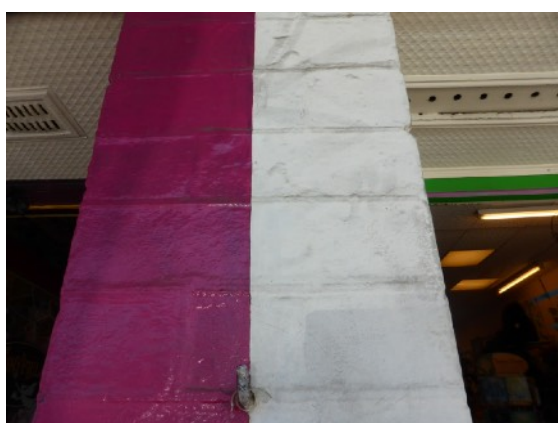


**Figure D8.** Pedestrian level view of the human scale of the historic shop fronts which are divided into narrow bays and deep ingoes, and fine decorative details.



**Figures D9 and 10.** Several shop fronts have historic lead light top lights in the colours and patterns of the Art Nouveau style.





**Figure D11.** Several shop fronts have tuck pointed brick work. Now overpainted, the delicate and skilled craft of tuck pointing can be revealed on the historic shopfronts by chemically removing the paint.



**Figure D12.** An example of the important visual link from the southern end of Station Street to the historic station buildings ( the signal box) and the historically open space of the railway reserve. When closer to the corner the station buildings can also be seen from Station Street. The Fido street sculpture illustrates the 'village square' function of the Railway Reserve.



**Figure D13.** Another example of the visual connection from Wingrove Street to the historically and aesthetically significant open space of the Railway Reserve and the historic railway places (station buildings on both sides of the tracks, pedestrian bridge and palm trees).



**Figure D14.** View from Gillies Street, along Wingrove St, with the historic space of the Railway Reserve on the right hand side of the street.



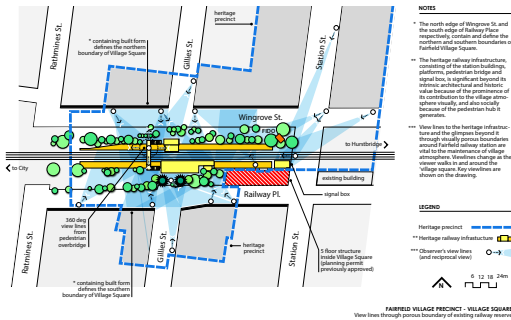
**Figure D15.** View of the north side Railway Station, and south side though the passage, as seen from Gillies St intersection with Wingrove St.



**Figure D16.** View along Wingrove Street from Station Street, of the north and south station buildings, set in the historically spacious railway reserve with one of the two palm trees on the south side, visible above the roof



**Figure D17.** View from Station Street showing the significant station features: signal box on the left, platforms, south and north station buildings and the timber pedestrian bridge.



**Figure D18.** Detail diagram of the 'Village Square', Fairfield Village Heritage Precinct developed and documented in Stage Two. See a larger version of the diagram at Figure 5.2.

## Comparative analysis

This is the only commercial heritage precinct in Fairfield which illustrate the development of Fairfield Village from the early stage in the late 1880s clustered around the Fairfield Railway station and reserve, and the extensive Inter-war development after electrification of the railway line in 1921 in Railway Place, Wingrove, and Station Streets. The integrity of the Fairfield Village precinct is high, with most of the early shops, bank, and church intact, with about a third being more recent low scale infill development.

Existing commercial heritage precincts in Darebin include High Street Reservoir, High Street Preston, Plenty Road Preston, High Street Northcote, High Street Thornbury and High Street Westgarth.

### *High Street Preston, (map)*

The High Street (Preston) precinct, comprising only one period of development which is represented by the inter-war commercial buildings at 274-88 and 317-41 High Street. It is much smaller and compared to the complexity of two major development phases of the Fairfield Village precinct centred around the railway station.

### *Plenty Road, Preston (map), and High Street Reservoir (map)*

Comprises late Victorian shops and residences and demonstrates the early development of Preston in the late nineteenth century, which was clustered in the area to the south of Bell Street. It also compares to the smaller High Street, Reservoir precinct and the following commercial precincts elsewhere in the City of Darebin.

### *High Street, Northcote*

Part of the High Street shopping centre in Northcote is included within the Heritage Overlay as **part of a broader precinct area**. The Northcote shopping centre comprises a mix of nineteenth century and some early twentieth century buildings with a more limited number of inter-war buildings. It therefore illustrates an earlier phase of development of Darebin. The integrity of the Northcote precinct is lower with a number of non-contributory buildings.

### *High Street, Westgarth*

This small centre, generally to the north of Westgarth Street is similarly **part of a broader HO precinct area**. It comprises an intact group of late nineteenth and early twentieth shops, which surround the landmark Westgarth Theatre. The development in this centre represents an earlier phase of development in Darebin City. The integrity of this centre is comparable to the High Street, Preston precinct.

### *High Street, Thornbury (map)*

The High Street, Thornbury precinct is a representative example of an inter-war shopping strip, which served a regional role. It provides evidence of the suburban expansion of Thornbury during



the 1920s development boom and how High Street was consolidated as one of the major shopping centres in the northern suburbs. As a representative example it demonstrates the strong associations between the development of commercial centres and public transport in the early to mid-twentieth century and how main centres serving a regional catchment were established along key public transport routes.

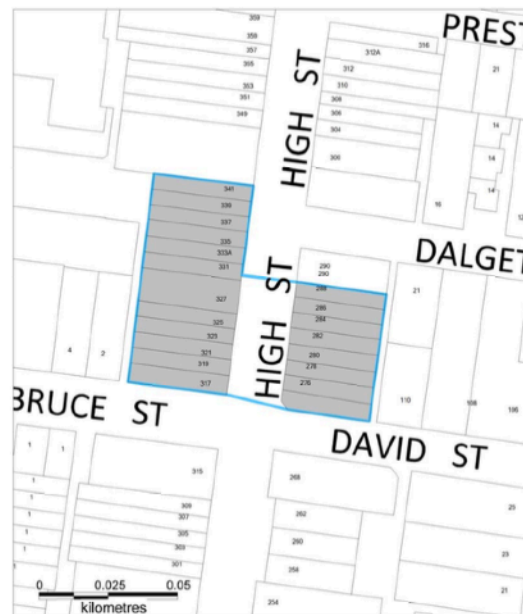
Comparatively, the Fairfield Village precinct most closely relates to the High Street Thornbury precinct in size and the association with the electrification of the railway, but Fairfield Village is the only precinct centred around the railway station, which was the original impetus for commercial and residential development in Fairfield in the late 1880s, and then in the Inter-war period, and it is more complex than the existing heritage precincts in Darebin because of the two development phases and the extension of the commercial area along Station St which is at right angles to the railway reserve and earliest development area. It is the largest commercial precinct representing the more modest development typical of a working man's area, in Darebin. It compares well in terms of its integrity with commercial precincts already in the HO although there is a larger proportion of non heritage places. As previously noted it illustrates two main historic eras of development in Fairfield, which are not represented by the other centres and, in particular, is important as an illustration of the commercial development in Fairfield Village.

## Comparative Analysis - Illustrations

Source: City of Darebin Heritage Study, Historic heritage places. 2011.



High Street Preston



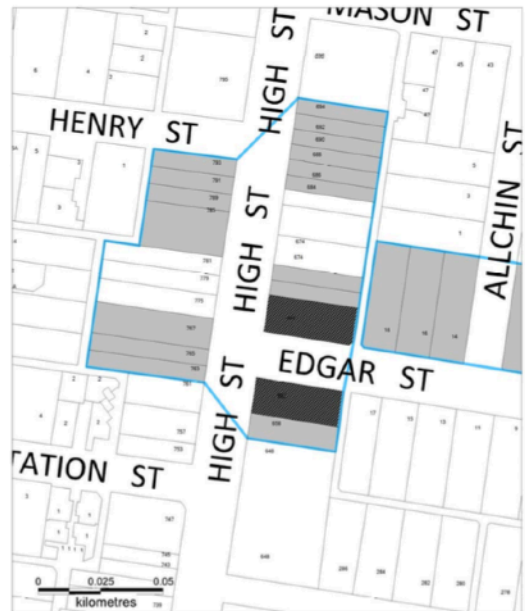




Plenty Road, Preston



High Street Reservoir





High Street Thornbury



## 5.2. Conservation Policy

### Objectives

To conserve the surviving views, and fabric that contributes to the Federation and Inter-war era appearance of the precinct when viewed from Railway Place, the railway reserve, Wingrove Street and Station Street.

To reveal the significance of the place by encouraging the restoration or reconstruction of missing features on all historic buildings listed in Fairfield Village Heritage Precinct Schedule of Places.

### Policy Basis

These historic buildings and views provide important evidence of the historic development of this area. New development may occur provided that it is carefully designed and does not impact upon the significance of the precinct. The upper facades of the shops are relatively intact externally and this contributes to the significance of the precinct, as does the surviving ground floor shopfronts, which are rare. The overall conservation objective therefore is to conserve and maintain the historic fabric, with an emphasis upon the elements that are visible from Railway Place, the railway reserve, Wingrove Street and Station Street.

### Guidelines (General)

In order to conserve the heritage significance of this place, it is policy to:

- Conserve the fabric of the building/s or other elements that contribute to the significance of the place and in particular to:
  - Encourage the accurate restoration or reconstruction of missing or modified elements on the basis of available evidence.
  - The removal of non-significant or intrusive elements, particularly where this would assist in revealing or interpreting the significance of the place.
- Encourage a contextual approach to new development that is complementary in form, scale and materials to the elements of significance.
- Ensure that new development does not become a visually dominant element as a result of its scale, form or siting; i.e. it should appear as a secondary element when compared to the original building. Refer to the Design Guidelines for more details.
- Discourage the demolition of elements of primary significance except where it can be demonstrated that:
  - The building is structurally unsound and cannot feasibly be repaired without undertaking significant alterations that would diminish the integrity of the building, and:
    - The proposed replacement building embodies design excellence that is complementary in form, scale and materials to the elements of significance.
  - Demolition of part of an element of primary significance may be allowed when it can be demonstrated to the satisfaction of the responsible authority that its demolition or removal will not adversely affect the significance of the place and, as appropriate:
    - It will assist in the long term conservation or maintenance of the place, or
    - It will support the viability of the existing use of the place or will facilitate a new use that is compatible with the on- going conservation of the building, or
    - It will upgrade the building to meet contemporary living standards such as improving energy efficiency.

- Consider the demolition or modification of elements of secondary significance only if the proposed replacement building embodies design excellence that is complementary in form, scale and materials to the elements of primary significance.

### Guidelines (Specific)

Whilst landowners are not obliged to undertake restoration works, these guidelines provide recommendations to facilitate the retention and enhancement of the culturally significant place, its fabric and its setting, when restoration works or alterations to the building are proposed. They also identify issues particular to the place and provide further detailed advice where relevant.

- Additions and new buildings
  - Refer to the Design Guidelines for Fairfield Village.
- Reconstruction and Restoration
  - Never sand, water or soda blast the building as this will permanently damage the bricks, mortar and render.
  - Do not paint or seal the brick work, glazed tiles, or brass/copper window frames.
  - Remove paint from bricks, render and tiles using chemicals, not sandblasting.
- Signage
  - Ensure all signage is designed to fit around the significant architectural design features, not over them.
- Railway Reserve
  - It is acknowledged that, in the future, it may become necessary for the level crossing to be removed. If this situation eventuates:
    - Efforts should be made to limit the impact on the heritage significance of the precinct and potentially enhance the heritage qualities in the design for any future grade separation works.
    - A design outcome should be provided which protects the spatial arrangement and form of buildings, as well as key open space view lines as primary requirements.
    - A new station on the east side of Station St may provide the opportunity to limit impact on the existing station buildings, providing:
      - the new design is complimentary to the heritage characteristics of the heritage precinct.
      - the existing significant railway places ( as defined in the Schedule) are protected, enhanced and maintained and able to be incorporated in a new use which does not significantly diminish the heritage significance of the place.

### Review of significance and policy

While every effort has been made to ensure that the information contained in this citation is accurate, it is possible that more detailed investigation may reveal further information about the significance of the place. For example, in most cases an internal inspection was not made of buildings at the time of initial assessment. In the time since the place was first assessed it is also possible that the condition of buildings or trees may have changed.



The information contained in this citation should therefore be reviewed at the time that it is proposed to make changes to the property. This would likely require a more detailed assessment of any significant or contributory element that is affected by any proposed buildings or works. Once this more detailed assessment has been made, a review of the significance of the place and the conservation policy should be carried out by Council's Heritage Adviser or an appropriately qualified professional.

### **5.3. Incorporated Plan for Permit Exemptions,**

Recommended amendments to the *City of Darebin Heritage Study Incorporated Plan - Permit Exemptions (2011)* have been made so that it can be applied to the outcomes of this study. (A copy of *City of Darebin Heritage Study Incorporated Plan - Permit Exemptions (2011) Recommended Amendments (2017)*, is in Appendix 8.2.)

## 6. Heritage Precinct Design Guidelines

### 6.1. Introduction

The Heritage Precinct Design Guidelines provides a discussion regarding measures which encourage the sensitive redevelopment of buildings and precincts with heritage value, through the retention of the 'village feel', historic views, streetscapes and historic facades. Appropriate setbacks and heights of additional levels and use of respectful design elements and materials within the proposed heritage precinct are illustrated in the architectural drawings in this section.

#### **Fairfield Village Heritage Precinct**

A single precinct was identified in this study as a high priority as it provides the opportunity for future development to enhance the contextual relationship and view lines of the heritage places within the streetscapes and railway reserve, and thus strengthen the 'village feel'. The contribution of views to, from and through the railway reserve particularly to Fairfield railway buildings, signal box, pedestrian bridge, palms and surrounds, in the proposed heritage precinct are noted to be of particular importance. There are 90 places in the precinct, of which 60 are heritage places and three of these have an individual heritage overlay.

### 6.2. Fairfield's 'Village Feel'

What is a village in this context? To what extent do the historic buildings contribute to the "village feel? To what extent does the skyline matter?

The 'village' is lined with buildings, infrastructure, community facilities and open spaces, particularly in the railway reserve. It is a self-identified precinct which is seen as a focus and an extension of the local community and in this way performs functions similar to that of a civic space, like town squares in old European cities. When these spaces work well, they serve as a public stage for our private lives. They can be the settings where celebrations are held, where social and economic exchanges take place - where friends run into each other, where cultures mix, where people meet and shop and eat in public. In a world of screen based, web interaction - these spaces are the last true "front porches" of our private lives where we physically interact with those living close to us - our geographical tribe. In short, they provide a sense of belonging.

It is because change is both an inevitable and an increasingly rapid process, the role that local governments have always had to nurture and promote these existing village precincts has become critical. Such areas must be safeguarded by means of local planning regulation. In cases like the proposed Fairfield Village Heritage Precinct, the context of the whole must be considered more important than any particular part (an individual development). It is vital that every development within the heritage precinct be seen as something that contributes to the local heritage environment as a whole in form, scale and character.

The Heritage Precinct Design Guidelines for the proposed heritage precinct must be able to withstand the pressure of ad hoc development proposals and a resultant ad-hoc planning approval process. It should be noted that the lack of a consistent set of existing guidelines at a whole-of-precinct level mean that council determinations are much more vulnerable to successful appeal at VCAT and overturned planning decisions. This creates disruption to the planning process, extra costs of defence, rapid degradation of the village scale and amenity and resultant public dissatisfaction.

### 6.3. Heritage Precinct Design Characteristics

Fairfield Village Commercial 1 zoned land, is a traditional narrow street-based centre, with a typical retail spine running approximately 500m in a north-south direction along Station Street, however, it also wraps around the spacious Fairfield Station area along Wingrove Street and Railway Place on either side of the railway line, providing a wide 'village square'.

It is noted that, within the proposed heritage precinct, that the traditional allotments have narrow street boundaries and the building forms are narrow, one or two storey buildings only, whereas, recent developments outside the proposed heritage precinct are significantly larger by comparison, particularly the floor area and heights, and they are wider in street frontage, and therefore they dominate the views to and within the commercial zone, over the smaller more human scale places. In some parts of the Commercial 1 zoned land, the large developments have reduced the strength of the visual connectedness of heritage places close to them. For example, as expert evidence to the 72A Railway Place VCAT hearing - (Dinopoulos v Darebin CC [2017] VCAT 118' VCAT P1228/2016) heritage expert Michael Taylor, acknowledged that the signal box within HO106 will be obscured from view from the south, particularly from the section of the village south of the railway line in Station St and Railway Place, but justified it by saying it will still be able to be viewed from the north. This line of argument could potentially be used to see one view at a time cut off by the next ad hoc development along railway land. It is for this reason that the boundary of the proposed heritage precinct does not include any of the large developments already built or approved. The location of these developments relative to the Commercial 1 zoned land and the proposed Stage 1 heritage precinct boundary, are shown on the map in Figure 3.10.

The suggested design guidelines for new development in the heritage precinct seek to protect the heritage values from such large scale developments. However, it is important to note that the proposed heritage precinct covers only about 50% of the Commercial 1 zoned land of Fairfield village which means it will not preclude more large developments over and above the 12 approved, within the other 50% of Fairfield village. Therefore the location of larger scale developments could occur at the north end of Station Street and south of the railway line along Station Street, and at the west ends of Railway Place (there is already one approved in that location) and the west end of Wingrove Street. This ensures that the obligations for the City of Darebin to fulfil the requirements of the State Planning Policy Framework (SPPF) regarding settlement (Clause 11), consolidation of site and empty sites (Clause 15.01-1 ) and promoting more residential development in or near activity centres (Clause 16) can be reasonably fulfilled. It also ensures there are sufficient opportunities in Fairfield Village for Council to meet its own Municipal Strategic Statement (MSS) objectives to encourage a significant proportion of new housing within or immediately adjacent to activity centres with superior access to Principal Public Transport Network (PPTN), generally within 400 metres or train station or tram route (Clause 21.03 -1).

#### **What's good about the existing character of Fairfield village and how does the proposed heritage precinct contribute ?**

The existing status of the whole of the Fairfield Village study area is a busy, diverse local centre that has the following valued characteristics:

- A 'village atmosphere' and diversity of shops.
- Low scale building character with one to two storey parapets and isolated higher forms.
- Many historic and moderately decorated building facades, interspersed with more contemporary simple forms.
- A vibrant traditional main street environment.
- Narrow allotments and shop fronts and regular vertical divisions in the streetscape elevation.

The (largely) inter-war historic commercial building stock in the proposed Fairfield Village heritage precinct provides a tangible sense of continuity. Because of their low rise structure, the articulation of the building facades, the decorative elements, varying pediments and broken skyline, they provide an interesting and diverse streetscape that is divided into parts which have a human scale.

The picturesque form of the railway structures within their spacious settings, provides a rare group of more complex Federation era forms, decoration, details and skylines which is showcased by views of them against the sky, compared with the loss this design quality by the design of the higher and wider 5 storey development at 78A Railway Place, hard against the signal box. This segmental diversity of the streetscape and its concomitant 'human scale' is responsible for a great deal of the character of the precinct. Failure to preserve this human scale of divided façade elements corresponding to the original allotments and replacement with large expanses of horizontal - spandrels, windows and walls which are visible from the street, will come at significant cost to the 'village feel'. Failure to regulate minimum setbacks according to development height will also come at significant cost. That cost is likely to be the feeling of coldness, social alienation, and ultimately, the loss of a community sense of belonging, and patronage.

## 6.4. State Planning Policy Framework

The proposed heritage precinct will positively contribute to the State Planning Policy Framework (SPPF Clause 15 Built Environment and Heritage) objectives, in particular relating to:

- The public realm: Protect and enhance main pedestrian spaces ( in and around the station reserve), streets (Station Street, Railway Place and Wingrove St), parks and walkways ( in and around the open spaces and walkways in and along the railway reserve)
- Landmarks, view and Vistas: (Protect and enhance the landmark qualities of the railway buildings, and views to and from them.)
- Heritage: New development should respect, not simply copy, historic precedents and create a worthy legacy for future generations. (Specified setbacks and heights, materials, articulation of facades and skylines, and forms will encourage this approach.)
- Clause 15.03-1

Heritage conservation seeks to identify, assess and document places of cultural heritage significance as a basis for their inclusion in the planning scheme, to retain those elements that contribute to the importance of the heritage place, and to encourage the conservation and restoration of contributory elements.



## 6.5. Some Notes on Recent Buildings and Alterations

This discussion refers to development of sites within the proposed heritage precinct boundary.

Three 'recent' developments, are all two storey, yet their design could be altered to be more sensitive to the identified heritage values of the proposed heritage precinct. (Figure 6.1)

The two storey additions/modifications housing Skin, Face, Body/ Pharmacy (112 Station St) and Quixley real estate (113 Station St) businesses are case studies in how their design is not sensitive to the design elements of the historic places in this precinct. It is emphasised that these places were designed prior to any requirement to understand the heritage values in the study area. Even though these buildings are only two storeys in height, the large, wide horizontal expanses of a single façade treatment either flat glass (as in Quigley Real Estate) or powder-coated metal louvres as in Skin, Face, Body/ Pharmacy are insensitive and entirely inappropriate. The design guidelines are formulated to ensure these design elements are not included in future developments or even new facades for these buildings, but rather they will be designed to enhance the heritage values of the heritage precinct.



**Figure 6.1.** Examples of 'recent' developments in Station St. Source. L. Huddle 2016

The red brick two storey building is an example of an infill building that has used many appropriate design elements to respect to the streetscape by stepping the parapet and articulating the wall of the first floor so that it is not all on one plane. However some may criticise it as rather art-deco pastiche. Minor changes could improve it by modifying the horizontal window components on either side and rendering sections of the wall to break up the expanse of dark red brickwork.



**Figure 6.2.** Examples of the impact on the three dimensional strength of the context of heritage places, from oblique views of 'streamlined horizontal skylines and large expanses of flat walls and colour of 'recent' development'. Source. L. Huddle 2016

The addition behind CH James cafe and function centre (86 Station St), has a large setback from the street boundary and it is only just visible when viewed front on from the other side of the street, but its contribution to enhancing the proposed heritage precinct, is compromised because of the oblique view looking southwest from in front of the existing church building which is also set back, and because of the very modern finish of large flat areas of colour, and the lack of surface articulation, as well as the strong flat black horizontal parapet. Design elements that would assist this new development to be more sensitive to the heritage values

of the precinct, the inter-war parapet of the single storey shop and the late Victorian/ Federation design of the church include: dividing the flat areas of parapet and side walls into more articulated bays, articulated skyline instead of the long flat sharp horizontal edges, and a hipped and or gabled roof clad in a pale grey textured material similar to a light grey Colorbond.

Figure 6.3 illustrates the way a traditional hipped or gable roof in a light grey colour contributes to the 'village feel' as it is a more picturesque skyline, even though the roof area is large, as it is less dominant than the sharp horizontal edges of the examples in more recent designs. The hipped or gable roof would raise the building level higher, but it would have less negative impact on the valued articulated skylines of the precinct. The skyline in Figure 6.3 can be compared with the impact of flat, sharp edged horizontal skylines on the 'recent' developments in Figure 6.4.

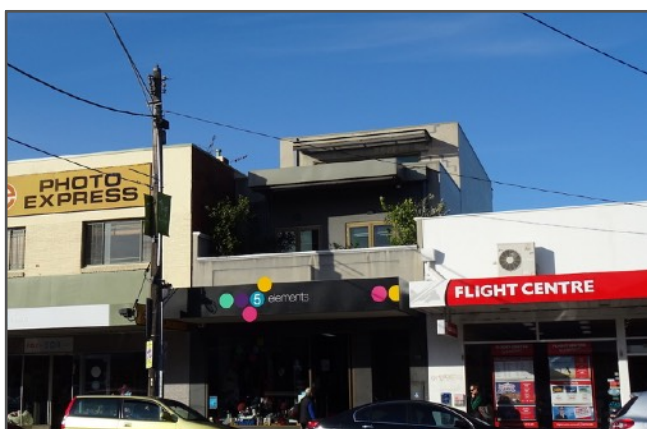
It is noted that the negative impact of oblique views of the sides of the sharp edged horizontal skylines on the 'recent' developments in Figure 6.4 will diminish as further development occurs on the infill sites and the gaps disappear. However, in special cases, such as adjacent to the church, or next to laneways, the side views will always remain visible, and thus the design of the sides is important.



**Figure 6.3.** Two examples of oblique views of heritage buildings which show the soft articulated skyline of traditional light grey hipped and gable roofs. Source. L. Huddle 2016



**Figure 6.4.** Three examples of oblique views of 'recent' developments which show the strong and dominant impact of sharp edged flat horizontal skylines. Source. L. Huddle 2016



**Figure 6.5.** A three storey infill example with substantial setbacks. Source. City of Darebin

Figure 6.5 above is an example of the 3 storey infill building with substantial setbacks for the second and third floors. This example works well in terms of the narrow street frontage and the ability of the ground floor to read as a separate single structure, which would be important for an addition to a single storey heritage building, and the solid to void ratio with vertical divisions is similar to the historic buildings. The design could fit better with the historic character of the heritage precinct if the parapet and fascias of the cantilevered verandas were articulated, rather than plain flat horizontal planes of colour. Although the oblique view of the large flat side of the building, (seen in Figure 6.4), has a negative impact on the heritage values of the precinct, it will be concealed when new development with the same setbacks is built adjacent to it.

## 6.6. Proposed Heights and Setbacks

It is recommended that different heights and setbacks for new development within the Heritage Precinct be applied, compared to the sites outside the Heritage Precinct, so that the special contribution of the single and two storey heritage places can be strengthened and enhanced.

In order to avoid 'façadism' (demolition of all structure behind the front elevation) whereby only the skin of the facade is retained, it is recommended to utilise a minimum of 4 metres of the existing structure and roofline behind the facade, (this might not always be feasible where insubstantial and/or non-complying alterations have been made to the existing structure).

It is recommended that the setbacks on Wingrove street, Railway Place and Station Street be identical, both with and without heritage places (infill Not-significant). Station Street is wider than Wingrove and Railway Place, but the latter two are also viewed from the railway reserve and from the other side of the railway, and so, on balance, it is recommended that a consistent approach to setbacks generally will promote the village scale and prevent oblique views of blank walls between developments which include valued heritage facades and those which do not. However, both of the corners of Wingrove and Station Streets are an exception, where the overall height is reduced and the setback of the top floor is reduced to provide a lower bookend 'mass'. Future developments either side would be subject to the previously determined setbacks. ( Figures 3.6 - 3.9 )

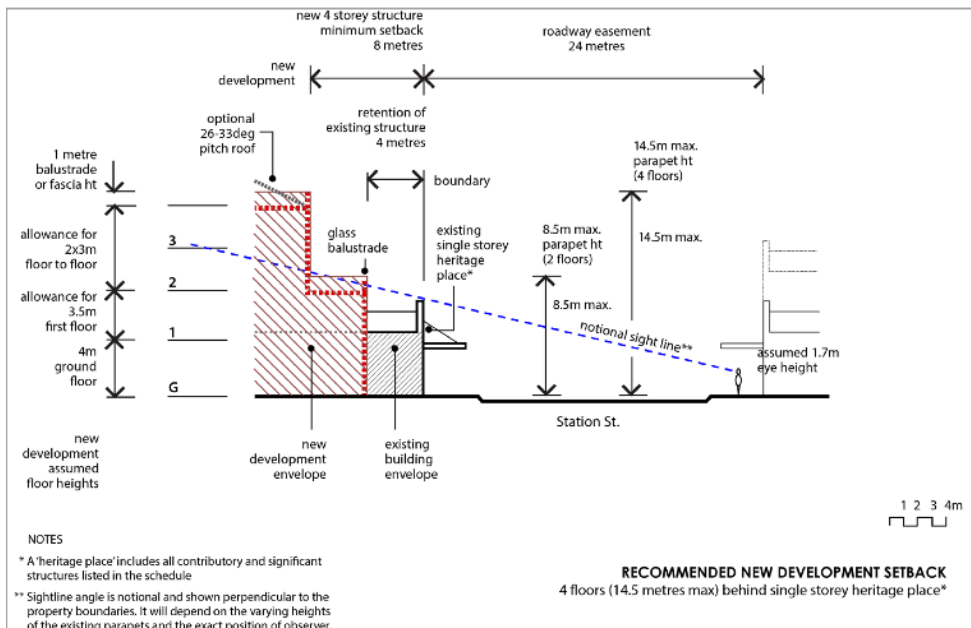
It should be noted that the recommended building envelope allows for floor to floor heights as follows: a commercial space/lobby of 4m on the ground floor, a first floor of up to 3.5m to allow for either residential or optional office space / mixed use accommodation, and 3m for all subsequent levels to accommodate residential apartments.

The hip or gable pitched roof of 26-33 degrees is an option that can be used instead of, or in addition to a parapet, and it should be in light grey materials so that it is a soft feature on the skyline. The drawings illustrate that the springing height of the pitched roof does not change the number of floors that can be built. i.e. maximum of 4 floors. It is anticipated that the roof space will be just roof space, but it could be used to increase the amenity of the top floor by increasing the ceiling height for the top floor only, by having a raked or coved ceiling.

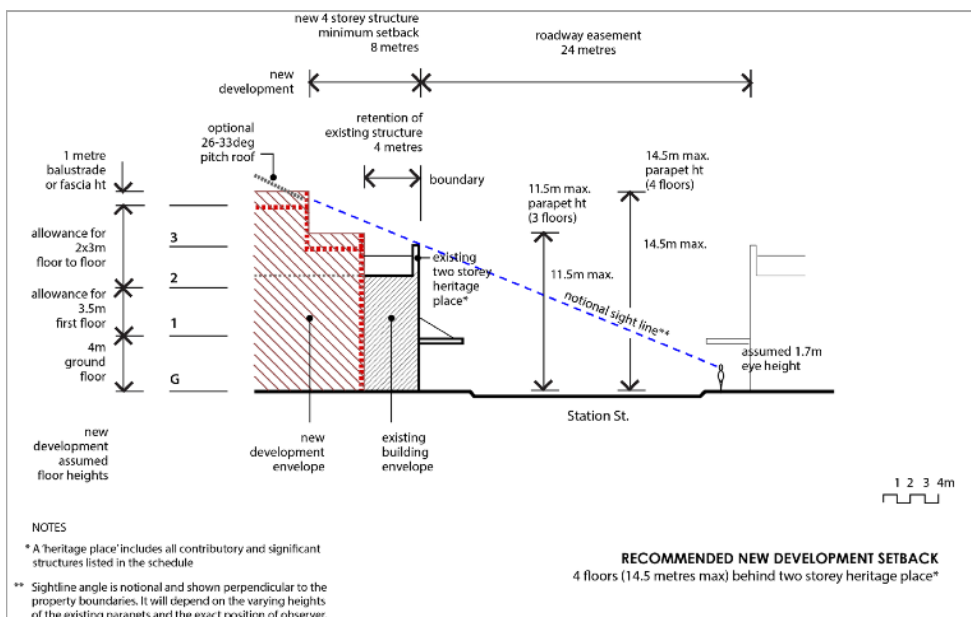
### 6.6.1. Cross-section drawings

Figures 6.6, 6.7, and 6.8 show recommended new development setbacks for three cases:

- Single storey heritage place - 4 floors (14.5 m max) behind the existing heritage place.
- Two storey heritage place - 4 floors (14m.5 max) behind the existing heritage place.
- Not-significant heritage place - 4 floors (14m.5 max) with infill.

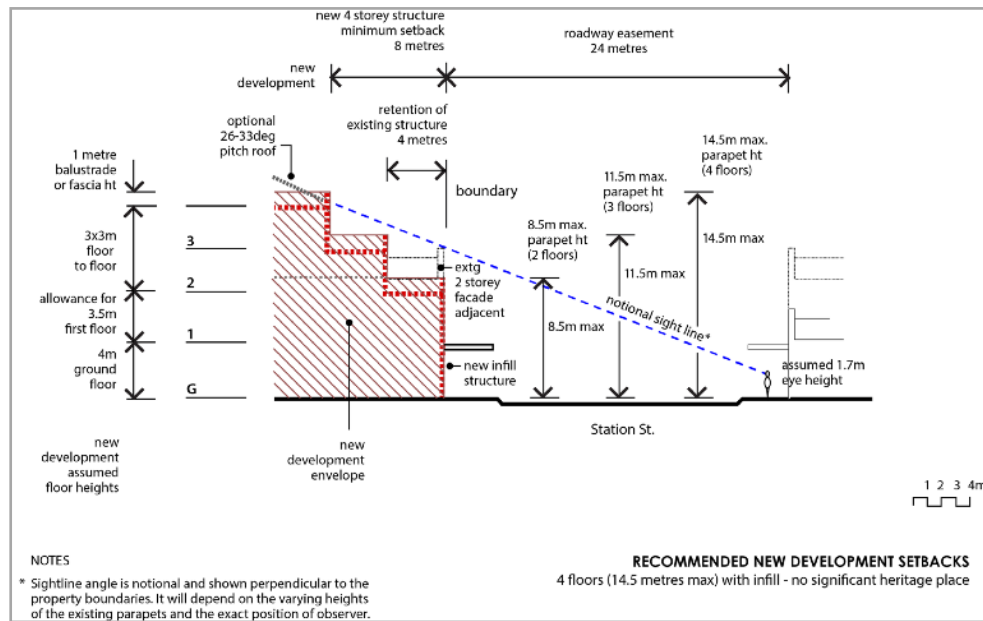


**Figure 6.6**  
Single storey heritage place - 4 floors (14m.5 max) behind the existing heritage place.



**Figure 6.7**  
Two storey heritage place - 4 floors (14m.5 max) behind the existing heritage place.

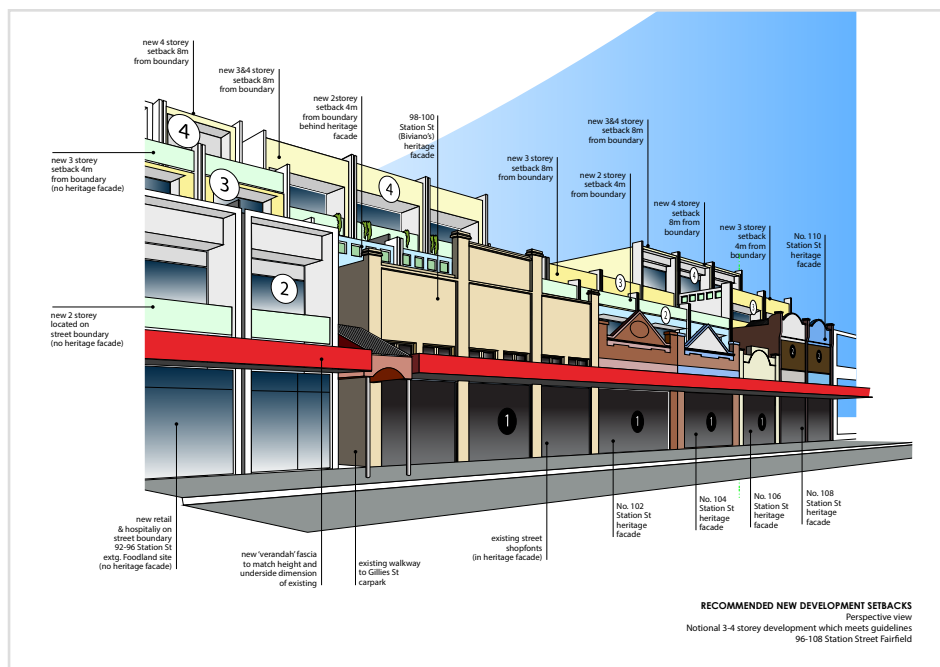
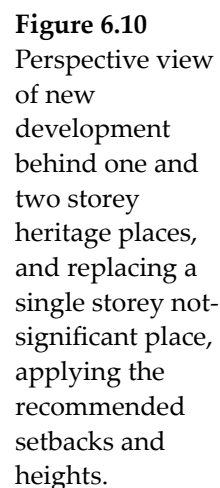




**Figure 6.8**  
Not-significant  
heritage place -  
4 floors (14.5 m  
max) with infill.

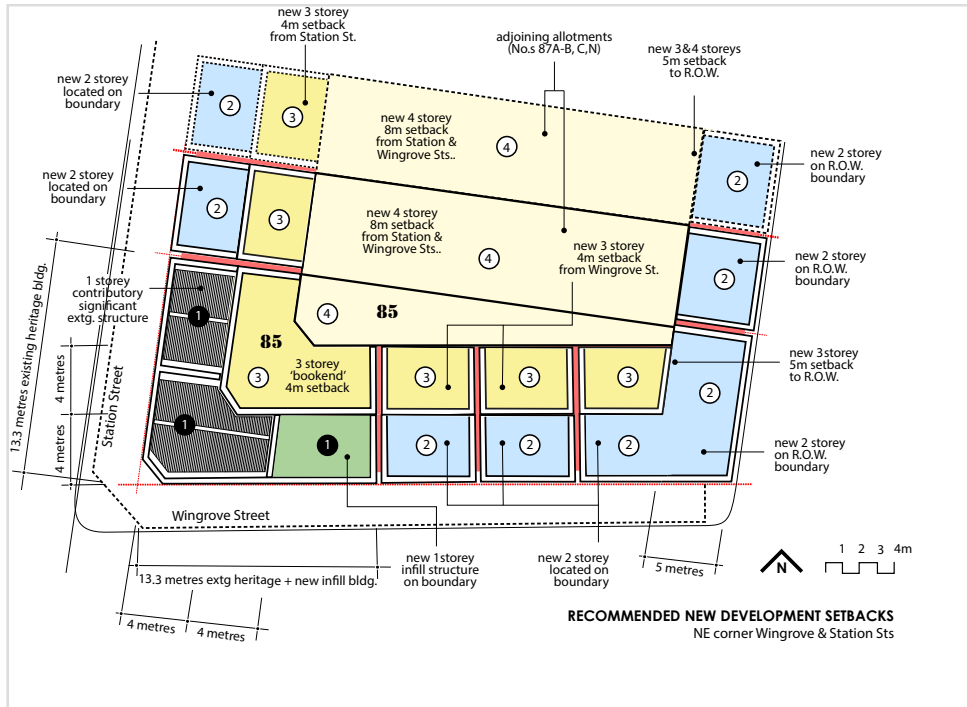
## 104/149

**Figure 6.9**  
Example of  
plans, elevations,  
and cross-  
sections new  
development  
applying the  
recommended  
setbacks and  
heights.



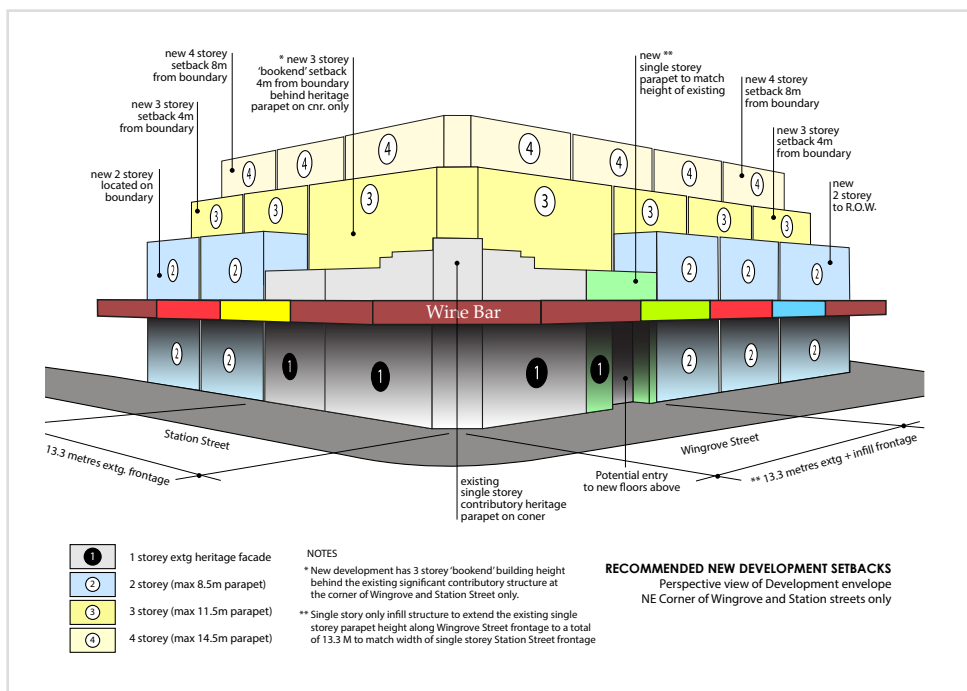
### 6.6.3. NE Corner Wingrove & Station Sts, Case Study Drawings

Figures 6.11 and 6.12, plans and perspective illustrate one example of how a variety of the recommended set backs and heights could be combined and how the new development could look. Note that the front elevations appear to be made up of narrow widths similar to the heritage places in the precinct.



**Figure 6.11**

Plan view showing the set backs for up to 4 storeys (14.5 m max)



**Figure 6.12**

A perspective view of possible development envelope for up to 4 storeys (14.5 m max)

### 6.6.4. NW Corner Wingrove & Station Sts, Case Study Drawings

Figures 6.13 and 6.14, plans and perspective illustrate one example of how a variety of the recommended set backs and heights could be combined and how the new development could look. Note that the front elevations appear to be made up of narrow widths similar to the heritage places in the precinct.

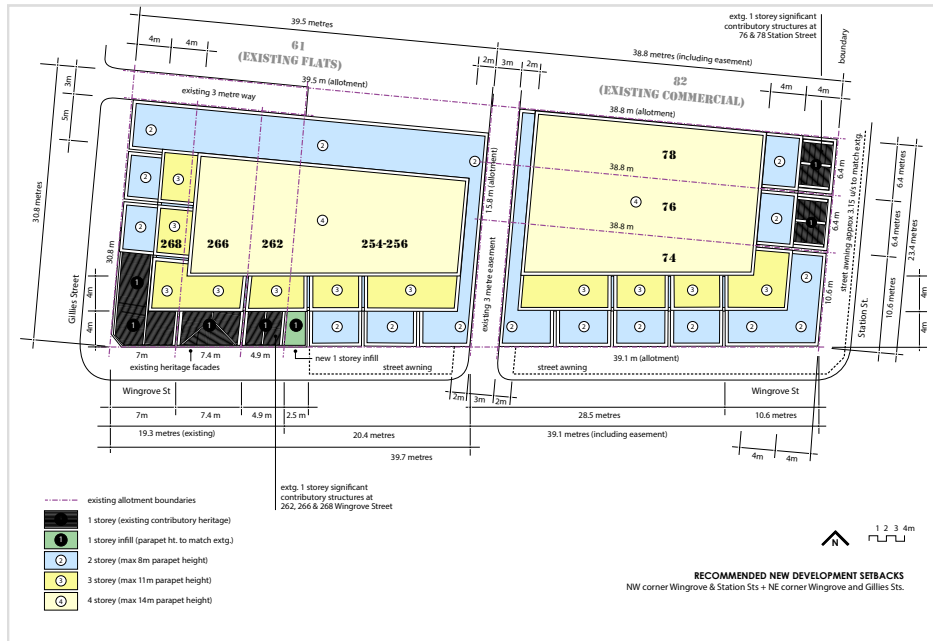


Figure 6.13

Plan view showing the setbacks for up to four storey development (14.5 m max) from the NW corner of Station St along Wingrove to Gillies St.

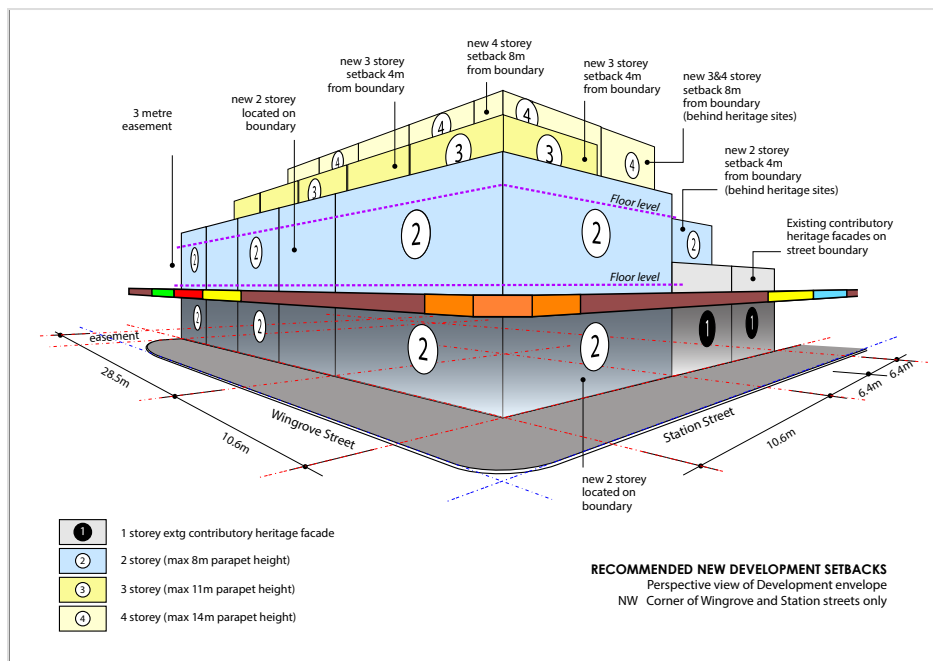


Figure 6.14

Perspective view of possible development envelope for up to 4 storey development (14.5 m max)



## 6.7. Architectural design techniques

It is also recommended that any new development, and development behind the street boundary of existing historic places or new infill have appropriate architectural design techniques to promote contemporary design that is sensitive to the existing features including:

- the option to have a light grey hipped or gable roof if it will be seen from the other side of the street, at an oblique angle eg beside a laneway.
- be concealed by an articulated parapet with fine grained detailing, including the skyline.
- clear glass balustrades behind a valued historic street parapet, to provide building regulation height for a deck in the set back for a new level without compromising the design of the parapet.
- specific location and sizes of signage on the verandahs, facades above ground floor level, and parapets.
- modern interpretation of decorative features to break up the facade to more human scale as seen in the historic facade, eg fine horizontal string courses, pilasters, use of cement rendered articulation, rectilinear window openings or round arched window openings with a vertical axis, 60:40 solid to void facades above ground level, decorative brick and render features to articulate the parapets and facades into smaller sizes.
- cantilevered verandahs extended to the kerb line.
- balance of vertical and horizontal lines over the facade and parapets (ie no wide flat areas, including spandrels)
- narrow shop fronts divided into smaller horizontal and vertical parts to provide human scale. (see illustrations below)
- Fairly consistent widths of narrow allotments with narrow shop fronts accented by the detailed design features.



- Larger allotments to reflect the common pattern of narrow allotments in the building design which divides the facade symmetrically and vertically into narrower bays.
- Symmetric designs.
- Articulated facades achieved with fine horizontal string courses, vertical and horizontal projecting and recessed panels/pilasters, etc.
- Parapet design to have rectilinear or curved skylines
- Shop fronts divided into smaller areas using stall boards at the ground level, vertical divisions such as mullions, and decorative lead light highlight windows.
- Cantilevered verandahs with narrow fascias to allow the decorative parapets to feature in the streetscape.
- Solid to void ratios at first floor level of approximately 60:40.

## 7. Recommendations

### 7.1. Introduction

This section provides key recommendations of the Study, which are considered to be fundamental to the achievement of an effective Heritage Strategy for the municipality.

The recommendations include:

- The adoption of the *Fairfield Village Heritage Assessment* (2017).
- The implementation of the *Fairfield Village Heritage Assessment* (2017), as listed in Section 7.3.
- Update the necessary Planning Policy, as listed in Section 7.3.

### 7.1. Adoption of Study

It is recommended that City of Darebin Council formally adopts the Fairfield Village Heritage Assessment (2017) Report.

### 7.2. Implementation of the Study

The study makes the following recommendations in order to implement the key findings of the final assessment.

#### 7.2.1. Planning Scheme Amendment

It is recommended that Darebin City Council implements the findings of this report by preparing an amendment to the Darebin Planning Scheme that will:

- Add the one new place 85-87 Gillies St, assessed as being of local significance as an individual place to the Heritage Overlay of the Darebin Planning Scheme. The extent of registration is the whole of the property boundary.
- Add the Fairfield Village Heritage Precinct assessed as being of local significance as an individual place to the Heritage Overlay of the Darebin Planning Scheme. The extent of registration is the whole of the precinct as shown in the precinct plan. The precinct identifies the Significant, Contributory and not-significant/non-contributory places.
- Make changes to the Municipal Strategic Statement and the Local Planning Policy Framework, as required.
- Add the incorporated document *City of Darebin Heritage Study Incorporated Plan - Permit Exemptions (2011) Recommended Amendments (2017)*
- Update HO190 in the Schedule to the HO of the Darebin Planning Scheme to include interior controls for the 1885 church.
- Remove HO106 from the Schedule to the HO of the Darebin Planning Scheme.
- Enter the individual place and one precinct into the Hermes Database using the documentation in this report update HO190 with the interior controls and remove HO106 from the Hermes Database.
- Make the final Assessment publicly available for download on Council's website.

## 8. Appendices

### 8.1. Draft Schedule to the Heritage Overlay

#### DAREBIN PLANNING SCHEME






##### SCHEDULE TO THE HERITAGE OVERLAY

The requirements of this overlay apply to both the heritage place and its associated land.

PS Map Ref	Heritage Place	External Paint Controls Apply?	Internal Alteration Controls Apply?	Tree Controls Apply?	Outbuildings or fences which are not exempt under Clause 43.01-3	Included on the Victorian Heritage Register under the Heritage Act 1995?	Prohibited uses may be permitted?	Name of Incorporated Plan under Clause 43.01-2	Aboriginal heritage place?
<b>FAIRFIELD</b>									
HOXX	St Andrew's <del>Alphington</del> and Fairfield Uniting Church 85 <del>Gillies</del> Street, Fairfield	Yes	Yes	No	No	No	No	City of Darebin Heritage Study Incorporated Plan - Permit Exemptions (2011) Recommended Amendments (2017)	No
HOXX	Fairfield Village Heritage Precinct Railway Place, Railway Reserve, Station Street, <del>Wingrove</del> Street, as shown in the precinct map.	Yes	No	Yes. Two palm trees on the railway reserve only.	No	No	No	City of Darebin Heritage Study Incorporated Plan - Permit Exemptions (2011) Recommended Amendments (2017)	No





KEY	
Fairfield Village Heritage Precinct.	
	Proposed Heritage Overlay Precinct boundary.
	Location of existing Heritage Overlay within the proposed precinct. HO190 church.
	Location of <i>Significant</i> heritage places within the proposed precinct.
	Location of <i>Contributory</i> heritage places within the proposed precinct.
	Location of not significant/not contributory places.

## 8.3. HERCON Criteria

### INFORMATION NOTE

### Heritage Council Criteria for the Assessment of Cultural Heritage Significance

#### Introduction

The following assessment criteria were adopted by the Heritage Council of Victoria at its meeting on 7 August 2008, and replace the previous criteria adopted by the Heritage Council on 6 March 1997.

#### Criteria

- Criterion A** Importance to the course, or pattern, of Victoria's cultural history
- Criterion B** Possession of uncommon, rare or endangered aspects of Victoria's cultural history.
- Criterion C** Potential to yield information that will contribute to an understanding of Victoria's cultural history.
- Criterion D** Importance in demonstrating the principal characteristics of a class of cultural places or objects.
- Criterion E** Importance in exhibiting particular aesthetic characteristics.
- Criterion F** Importance in demonstrating a high degree of creative or technical achievement at a particular period.
- Criterion G** Strong or special association with a particular community or cultural group for social, cultural or spiritual reasons. This includes the significance of a place to Indigenous peoples as part of their continuing and developing cultural traditions.
- Criterion H** Special association with the life or works of a person, or group of persons, of importance in Victoria's history.

HERITAGE  
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**Victoria** Department of Planning  
and Community Development  
The Place To Be

Prepared by Heritage Victoria for the  
Heritage Council of Victoria.

Published by the Heritage Council of  
Victoria, August 2008.

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## **8.4. Burra Charter**

# THE BURRA CHARTER

The Australia ICOMOS Charter for  
Places of Cultural Significance

2013



Australia ICOMOS Incorporated  
International Council on Monuments and Sites



## ICOMOS

ICOMOS (International Council on Monuments and Sites) is a non-governmental professional organisation formed in 1965, with headquarters in Paris. ICOMOS is primarily concerned with the philosophy, terminology, methodology and techniques of cultural heritage conservation. It is closely linked to UNESCO, particularly in its role under the World Heritage Convention 1972 as UNESCO's principal adviser on cultural matters related to World Heritage. The 11,000 members of ICOMOS include architects, town planners, demographers, archaeologists, geographers, historians, conservators, anthropologists, scientists, engineers and heritage administrators. Members in the 103 countries belonging to ICOMOS are formed into National Committees and participate in a range of conservation projects, research work, intercultural exchanges and cooperative activities. ICOMOS also has 27 International Scientific Committees that focus on particular aspects of the conservation field. ICOMOS members meet triennially in a General Assembly.

## Australia ICOMOS

The Australian National Committee of ICOMOS (Australia ICOMOS) was formed in 1976. It elects an Executive Committee of 15 members, which is responsible for carrying out national programs and participating in decisions of ICOMOS as an international organisation. It provides expert advice as required by ICOMOS, especially in its relationship with the World Heritage Committee. Australia ICOMOS acts as a national and international link between public authorities, institutions and individuals involved in the study and conservation of all places of cultural significance. Australia ICOMOS members participate in a range of conservation activities including site visits, training, conferences and meetings.

## Revision of the Burra Charter

The Burra Charter was first adopted in 1979 at the historic South Australian mining town of Burra. Minor revisions were made in 1981 and 1988, with more substantial changes in 1999.

Following a review this version was adopted by Australia ICOMOS in October 2013.

The review process included replacement of the 1988 Guidelines to the Burra Charter with Practice Notes which are available at: [australia.icomos.org](http://australia.icomos.org)

Australia ICOMOS documents are periodically reviewed and we welcome any comments.

## Citing the Burra Charter

The full reference is *The Burra Charter: The Australia ICOMOS Charter for Places of Cultural Significance, 2013*. Initial textual references should be in the form of the *Australia ICOMOS Burra Charter, 2013* and later references in the short form (*Burra Charter*).

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The Burra Charter consists of the Preamble, Articles, Explanatory Notes and the flow chart.

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Cover photograph by Ian Stapleton.

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ISBN 0 9578528 4 3

# The Burra Charter

(The Australia ICOMOS Charter for Places of Cultural Significance, 2013)

## Preamble

Considering the International Charter for the Conservation and Restoration of Monuments and Sites (Venice 1964), and the Resolutions of the 5th General Assembly of the International Council on Monuments and Sites (ICOMOS) (Moscow 1978), the Burra Charter was adopted by Australia ICOMOS (the Australian National Committee of ICOMOS) on 19 August 1979 at Burra, South Australia. Revisions were adopted on 23 February 1981, 23 April 1988, 26 November 1999 and 31 October 2013.

The Burra Charter provides guidance for the conservation and management of places of cultural significance (cultural heritage places), and is based on the knowledge and experience of Australia ICOMOS members.

Conservation is an integral part of the management of places of cultural significance and is an ongoing responsibility.

Who is the Charter for?

The Charter sets a standard of practice for those who provide advice, make decisions about, or undertake works to places of cultural significance, including owners, managers and custodians.

Using the Charter

The Charter should be read as a whole. Many articles are interdependent.

The Charter consists of:

- Definitions Article 1
- Conservation Principles Articles 2–13
- Conservation Processes Articles 14–25
- Conservation Practices Articles 26–34
- The Burra Charter Process flow chart.

The key concepts are included in the Conservation Principles section and these are further developed in the Conservation Processes and Conservation Practice sections. The flow chart explains the Burra Charter Process (Article 6) and is an integral part of

the Charter. Explanatory Notes also form part of the Charter.

The Charter is self-contained, but aspects of its use and application are further explained, in a series of Australia ICOMOS Practice Notes, in *The Illustrated Burra Charter*, and in other guiding documents available from the Australia ICOMOS web site: [australia.icomos.org](http://australia.icomos.org).

What places does the Charter apply to?

The Charter can be applied to all types of places of cultural significance including natural, Indigenous and historic places with cultural values.

The standards of other organisations may also be relevant. These include the *Australian Natural Heritage Charter*, *Ask First: a guide to respecting Indigenous heritage places and values* and *Significance 2.0: a guide to assessing the significance of collections*.

National and international charters and other doctrine may be relevant. See [australia.icomos.org](http://australia.icomos.org).

Why conserve?

Places of cultural significance enrich people's lives, often providing a deep and inspirational sense of connection to community and landscape, to the past and to lived experiences. They are historical records, that are important expressions of Australian identity and experience. Places of cultural significance reflect the diversity of our communities, telling us about who we are and the past that has formed us and the Australian landscape. They are irreplaceable and precious.

These places of cultural significance must be conserved for present and future generations in accordance with the principle of inter-generational equity.

The Burra Charter advocates a cautious approach to change: do as much as necessary to care for the place and to make it useable, but otherwise change it as little as possible so that its cultural significance is retained.

## Article 1. Definitions

For the purposes of this Charter:

- 1.1 *Place* means a geographically defined area. It may include elements, objects, spaces and views. Place may have tangible and intangible dimensions.
- 1.2 *Cultural significance* means aesthetic, historic, scientific, social or spiritual value for past, present or future generations.  
  
Cultural significance is embodied in the *place* itself, its *fabric*, *setting*, *use*, *associations*, *meanings*, *records*, *related places* and *related objects*.  
  
Places may have a range of values for different individuals or groups.
- 1.3 *Fabric* means all the physical material of the *place* including elements, fixtures, contents and objects.
- 1.4 *Conservation* means all the processes of looking after a *place* so as to retain its *cultural significance*.
- 1.5 *Maintenance* means the continuous protective care of a *place*, and its *setting*.  
  
Maintenance is to be distinguished from repair which involves *restoration* or *reconstruction*.
- 1.6 *Preservation* means maintaining a *place* in its existing state and retarding deterioration.
- 1.7 *Restoration* means returning a *place* to a known earlier state by removing accretions or by reassembling existing elements without the introduction of new material.
- 1.8 *Reconstruction* means returning a *place* to a known earlier state and is distinguished from *restoration* by the introduction of new material.
- 1.9 *Adaptation* means changing a *place* to suit the existing *use* or a proposed use.
- 1.10 *Use* means the functions of a *place*, including the activities and traditional and customary practices that may occur at the place or are dependent on the place.

Place has a broad scope and includes natural and cultural features. Place can be large or small: for example, a memorial, a tree, an individual building or group of buildings, the location of an historical event, an urban area or town, a cultural landscape, a garden, an industrial plant, a shipwreck, a site with in situ remains, a stone arrangement, a road or travel route, a community meeting place, a site with spiritual or religious connections.

The term cultural significance is synonymous with cultural heritage significance and cultural heritage value.

Cultural significance may change over time and with use.

Understanding of cultural significance may change as a result of new information.

Fabric includes building interiors and sub-surface remains, as well as excavated material.

Natural elements of a place may also constitute fabric. For example the rocks that signify a Dreaming place.

Fabric may define spaces and views and these may be part of the significance of the place.

See also Article 14.

Examples of protective care include:

- maintenance — regular inspection and cleaning of a place, e.g. mowing and pruning in a garden;
- repair involving restoration — returning dislodged or relocated fabric to its original location e.g. loose roof gutters on a building or displaced rocks in a stone bora ring;
- repair involving reconstruction — replacing decayed fabric with new fabric

It is recognised that all places and their elements change over time at varying rates.

New material may include recycled material salvaged from other places. This should not be to the detriment of any place of cultural significance.

Use includes for example cultural practices commonly associated with Indigenous peoples such as ceremonies, hunting and fishing, and fulfillment of traditional obligations. Exercising a right of access may be a use.

## Articles

- 1.11 *Compatible use* means a *use* which respects the *cultural significance* of a *place*. Such a use involves no, or minimal, impact on cultural significance.
- 1.12 *Setting* means the immediate and extended environment of a *place* that is part of or contributes to its *cultural significance* and distinctive character.
- 1.13 *Related place* means a *place* that contributes to the *cultural significance* of another place.
- 1.14 *Related object* means an object that contributes to the *cultural significance* of a *place* but is not at the place.
- 1.15 *Associations* mean the connections that exist between people and a *place*.
- 1.16 *Meanings* denote what a *place* signifies, indicates, evokes or expresses to people.
- 1.17 *Interpretation* means all the ways of presenting the *cultural significance* of a *place*.

## Conservation Principles

### Article 2. Conservation and management

- 2.1 *Places of cultural significance* should be conserved.
- 2.2 The aim of *conservation* is to retain the *cultural significance* of a *place*.
- 2.3 *Conservation* is an integral part of good management of *places of cultural significance*.
- 2.4 *Places of cultural significance* should be safeguarded and not put at risk or left in a vulnerable state.

### Article 3. Cautious approach

- 3.1 *Conservation* is based on a respect for the existing *fabric, use, associations* and *meanings*. It requires a cautious approach of changing as much as necessary but as little as possible.
- 3.2 Changes to a *place* should not distort the physical or other evidence it provides, nor be based on conjecture.

### Article 4. Knowledge, skills and techniques

- 4.1 *Conservation* should make use of all the knowledge, skills and disciplines which can contribute to the study and care of the *place*.

## Explanatory Notes

Setting may include: structures, spaces, land, water and sky; the visual setting including views to and from the place, and along a cultural route; and other sensory aspects of the setting such as smells and sounds. Setting may also include historical and contemporary relationships, such as use and activities, social and spiritual practices, and relationships with other places, both tangible and intangible.

Objects at a place are encompassed by the definition of place, and may or may not contribute to its cultural significance.

Associations may include social or spiritual values and cultural responsibilities for a place.

Meanings generally relate to intangible dimensions such as symbolic qualities and memories.

Interpretation may be a combination of the treatment of the fabric (e.g. maintenance, restoration, reconstruction); the use of and activities at the place; and the use of introduced explanatory material.

The traces of additions, alterations and earlier treatments to the fabric of a place are evidence of its history and uses which may be part of its significance. Conservation action should assist and not impede their understanding.



## Articles

- 4.2 Traditional techniques and materials are preferred for the *conservation* of significant *fabric*. In some circumstances modern techniques and materials which offer substantial conservation benefits may be appropriate.

## Article 5. Values

- 5.1 *Conservation* of a *place* should identify and take into consideration all aspects of cultural and natural significance without unwarranted emphasis on any one value at the expense of others.
- 5.2 Relative degrees of *cultural significance* may lead to different *conservation* actions at a place.

## Article 6. Burra Charter Process

- 6.1 The *cultural significance* of a *place* and other issues affecting its future are best understood by a sequence of collecting and analysing information before making decisions. Understanding cultural significance comes first, then development of policy and finally management of the place in accordance with the policy. This is the Burra Charter Process.
- 6.2 Policy for managing a *place* must be based on an understanding of its *cultural significance*.
- 6.3 Policy development should also include consideration of other factors affecting the future of a *place* such as the owner's needs, resources, external constraints and its physical condition.
- 6.4 In developing an effective policy, different ways to retain *cultural significance* and address other factors may need to be explored.
- 6.5 Changes in circumstances, or new information or perspectives, may require reiteration of part or all of the Burra Charter Process.

## Article 7. Use

- 7.1 Where the *use* of a *place* is of *cultural significance* it should be retained.
- 7.2 A *place* should have a *compatible use*.

## Explanatory Notes

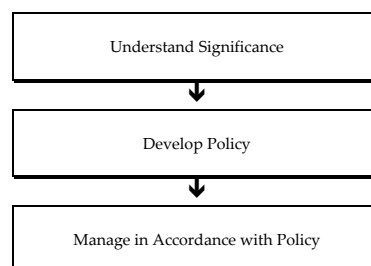
The use of modern materials and techniques must be supported by firm scientific evidence or by a body of experience.

Conservation of places with natural significance is explained in the Australian Natural Heritage Charter. This Charter defines natural significance to mean the importance of ecosystems, biodiversity and geodiversity for their existence value or for present or future generations, in terms of their scientific, social, aesthetic and life-support value.

In some cultures, natural and cultural values are indivisible.

A cautious approach is needed, as understanding of cultural significance may change. This article should not be used to justify actions which do not retain cultural significance.

The Burra Charter Process, or sequence of investigations, decisions and actions, is illustrated below and in more detail in the accompanying flow chart which forms part of the Charter.



Options considered may include a range of uses and changes (e.g. adaptation) to a place.

The policy should identify a use or combination of uses or constraints on uses that retain the cultural significance of the place. New use of a place should involve minimal change to significant fabric and use; should respect associations and meanings; and where appropriate should provide for continuation of activities and practices which contribute to the cultural significance of the place.

## Articles

### Article 8. Setting

*Conservation* requires the retention of an appropriate *setting*. This includes retention of the visual and sensory setting, as well as the retention of spiritual and other cultural relationships that contribute to the *cultural significance* of the *place*.

New construction, demolition, intrusions or other changes which would adversely affect the setting or relationships are not appropriate.

### Article 9. Location

- 9.1 The physical location of a *place* is part of its *cultural significance*. A building, work or other element of a place should remain in its historical location. Relocation is generally unacceptable unless this is the sole practical means of ensuring its survival.
- 9.2 Some buildings, works or other elements of *places* were designed to be readily removable or already have a history of relocation. Provided such buildings, works or other elements do not have significant links with their present location, removal may be appropriate.
- 9.3 If any building, work or other element is moved, it should be moved to an appropriate location and given an appropriate *use*. Such action should not be to the detriment of any *place* of *cultural significance*.

### Article 10. Contents

Contents, fixtures and objects which contribute to the *cultural significance* of a *place* should be retained at that place. Their removal is unacceptable unless it is: the sole means of ensuring their security and *preservation*; on a temporary basis for treatment or exhibition; for cultural reasons; for health and safety; or to protect the place. Such contents, fixtures and objects should be returned where circumstances permit and it is culturally appropriate.

### Article 11. Related places and objects

The contribution which *related places* and *related objects* make to the *cultural significance* of the *place* should be retained.

### Article 12. Participation

*Conservation*, *interpretation* and management of a *place* should provide for the participation of people for whom the place has significant *associations* and *meanings*, or who have social, spiritual or other cultural responsibilities for the place.

### Article 13. Co-existence of cultural values

Co-existence of cultural values should always be recognised, respected and encouraged. This is especially important in cases where they conflict.

## Explanatory Notes

Setting is explained in Article 1.12.

For example, the repatriation (returning) of an object or element to a place may be important to Indigenous cultures, and may be essential to the retention of its cultural significance.

Article 28 covers the circumstances where significant fabric might be disturbed, for example, during archaeological excavation.

Article 33 deals with significant fabric that has been removed from a place.

For some places, conflicting cultural values may affect policy development and management decisions. In Article 13, the term cultural values refers to those beliefs which are important to a cultural group, including but not limited to political, religious, spiritual and moral beliefs. This is broader than values associated with cultural significance.

# Conservation Processes

## Article 14. Conservation processes

*Conservation* may, according to circumstance, include the processes of: retention or reintroduction of a *use*; retention of *associations* and *meanings*; *maintenance*, *preservation*, *restoration*, *reconstruction*, *adaptation* and *interpretation*; and will commonly include a combination of more than one of these. Conservation may also include retention of the contribution that *related places* and *related objects* make to the *cultural significance* of a *place*.

## Article 15. Change

15.1 Change may be necessary to retain *cultural significance*, but is undesirable where it reduces cultural significance. The amount of change to a *place* and its *use* should be guided by the *cultural significance* of the place and its appropriate *interpretation*.

15.2 Changes which reduce *cultural significance* should be reversible, and be reversed when circumstances permit.

15.3 Demolition of significant *fabric* of a *place* is generally not acceptable. However, in some cases minor demolition may be appropriate as part of *conservation*. Removed significant fabric should be reinstated when circumstances permit.

15.4 The contributions of all aspects of *cultural significance* of a *place* should be respected. If a place includes *fabric*, *uses*, *associations* or *meanings* of different periods, or different aspects of cultural significance, emphasising or interpreting one period or aspect at the expense of another can only be justified when what is left out, removed or diminished is of slight cultural significance and that which is emphasised or interpreted is of much greater cultural significance.

## Article 16. Maintenance

*Maintenance* is fundamental to *conservation*. Maintenance should be undertaken where *fabric* is of *cultural significance* and its maintenance is necessary to retain that *cultural significance*.

## Article 17. Preservation

*Preservation* is appropriate where the existing *fabric* or its condition constitutes evidence of *cultural significance*, or where insufficient evidence is available to allow other *conservation* processes to be carried out.

Conservation normally seeks to slow deterioration unless the significance of the place dictates otherwise. There may be circumstances where no action is required to achieve conservation.

When change is being considered, including for a temporary use, a range of options should be explored to seek the option which minimises any reduction to its cultural significance.

It may be appropriate to change a place where this reflects a change in cultural meanings or practices at the place, but the significance of the place should always be respected.

Reversible changes should be considered temporary. Non-reversible change should only be used as a last resort and should not prevent future conservation action.

Maintaining a place may be important to the fulfilment of traditional laws and customs in some Indigenous communities and other cultural groups.

Preservation protects fabric without obscuring evidence of its construction and use. The process should always be applied:

- where the evidence of the fabric is of such significance that it should not be altered; or
- where insufficient investigation has been carried out to permit policy decisions to be taken in accord with Articles 26 to 28.

New work (e.g. stabilisation) may be carried out in association with preservation when its purpose is the physical protection of the fabric and when it is consistent with Article 22.

## Articles

### Article 18. Restoration and reconstruction

*Restoration and reconstruction* should reveal culturally significant aspects of the *place*.

### Article 19. Restoration

*Restoration* is appropriate only if there is sufficient evidence of an earlier state of the *fabric*.

### Article 20. Reconstruction

20.1 *Reconstruction* is appropriate only where a *place* is incomplete through damage or alteration, and only where there is sufficient evidence to reproduce an earlier state of the *fabric*. In some cases, reconstruction may also be appropriate as part of a *use* or practice that retains the *cultural significance* of the place.

20.2 *Reconstruction* should be identifiable on close inspection or through additional *interpretation*.

### Article 21. Adaptation

21.1 *Adaptation* is acceptable only where the adaptation has minimal impact on the *cultural significance* of the *place*.

21.2 *Adaptation* should involve minimal change to significant *fabric*, achieved only after considering alternatives.

### Article 22. New work

22.1 New work such as additions or other changes to the *place* may be acceptable where it respects and does not distort or obscure the *cultural significance* of the place, or detract from its *interpretation* and appreciation.

22.2 New work should be readily identifiable as such, but must respect and have minimal impact on the *cultural significance* of the *place*.

### Article 23. Retaining or reintroducing use

Retaining, modifying or reintroducing a significant *use* may be appropriate and preferred forms of *conservation*.

### Article 24. Retaining associations and meanings

24.1 Significant *associations* between people and a *place* should be respected, retained and not obscured. Opportunities for the *interpretation*, commemoration and celebration of these associations should be investigated and implemented.

24.2 Significant *meanings*, including spiritual values, of a *place* should be respected. Opportunities for the continuation or revival of these meanings should be investigated and implemented.

## Explanatory Notes

Places with social or spiritual value may warrant reconstruction, even though very little may remain (e.g. only building footings or tree stumps following fire, flood or storm). The requirement for sufficient evidence to reproduce an earlier state still applies.

Adaptation may involve additions to the place, the introduction of new services, or a new use, or changes to safeguard the place. Adaptation of a place for a new use is often referred to as 'adaptive re-use' and should be consistent with Article 7.2.

New work should respect the significance of a place through consideration of its siting, bulk, form, scale, character, colour, texture and material. Imitation should generally be avoided.

New work should be consistent with Articles 3, 5, 8, 15, 21 and 22.1.

These may require changes to significant fabric but they should be minimised. In some cases, continuing a significant use, activity or practice may involve substantial new work.

For many places associations will be linked to aspects of use, including activities and practices.

Some associations and meanings may not be apparent and will require research.



## Article 25. Interpretation

The *cultural significance* of many *places* is not readily apparent, and should be explained by *interpretation*. Interpretation should enhance understanding and engagement, and be culturally appropriate.

## Conservation Practice

## Article 26. Applying the Burra Charter Process

26.1 Work on a *place* should be preceded by studies to understand the place which should include analysis of physical, documentary, oral and other evidence, drawing on appropriate knowledge, skills and disciplines.

26.2 Written statements of *cultural significance* and policy for the *place* should be prepared, justified and accompanied by supporting evidence. The statements of significance and policy should be incorporated into a management plan for the place.

26.3 Groups and individuals with *associations* with the *place* as well as those involved in its management should be provided with opportunities to contribute to and participate in identifying and understanding the *cultural significance* of the place. Where appropriate they should also have opportunities to participate in its *conservation* and management.

26.4 Statements of *cultural significance* and policy for the *place* should be periodically reviewed, and actions and their consequences monitored to ensure continuing appropriateness and effectiveness.

## Article 27. Managing change

27.1 The impact of proposed changes, including incremental changes, on the *cultural significance* of a *place* should be assessed with reference to the statement of significance and the policy for managing the place. It may be necessary to modify proposed changes to better retain cultural significance.

27.2 Existing *fabric*, *use*, *associations* and *meanings* should be adequately recorded before and after any changes are made to the *place*.

## Article 28. Disturbance of fabric

28.1 Disturbance of significant *fabric* for study, or to obtain evidence, should be minimised. Study of a *place* by any disturbance of the fabric, including archaeological excavation, should only be undertaken to provide data essential for decisions on the *conservation* of the place, or to obtain important evidence about to be lost or made inaccessible.

In some circumstances any form of interpretation may be culturally inappropriate.

The results of studies should be kept up to date, regularly reviewed and revised as necessary.

Policy should address all relevant issues, e.g. use, interpretation, management and change.

A management plan is a useful document for recording the Burra Charter Process, i.e. the steps in planning for and managing a place of cultural significance (Article 6.1 and flow chart). Such plans are often called conservation management plans and sometimes have other names.

The management plan may deal with other matters related to the management of the place.

Monitor actions taken in case there are also unintended consequences.

28.2 Investigation of a *place* which requires disturbance of the *fabric*, apart from that necessary to make decisions, may be appropriate provided that it is consistent with the policy for the place. Such investigation should be based on important research questions which have potential to substantially add to knowledge, which cannot be answered in other ways and which minimises disturbance of significant fabric.

#### Article 29. Responsibility

The organisations and individuals responsible for management and decisions should be named and specific responsibility taken for each decision.

#### Article 30. Direction, supervision and implementation

Competent direction and supervision should be maintained at all stages, and any changes should be implemented by people with appropriate knowledge and skills.

#### Article 31. Keeping a log

New evidence may come to light while implementing policy or a plan for a *place*. Other factors may arise and require new decisions. A log of new evidence and additional decisions should be kept.

New decisions should respect and have minimal impact on the cultural significance of the place.

#### Article 32. Records

32.1 The records associated with the *conservation* of a *place* should be placed in a permanent archive and made publicly available, subject to requirements of security and privacy, and where this is culturally appropriate.

32.2 Records about the history of a *place* should be protected and made publicly available, subject to requirements of security and privacy, and where this is culturally appropriate.

#### Article 33. Removed fabric

Significant *fabric* which has been removed from a *place* including contents, fixtures and objects, should be catalogued, and protected in accordance with its *cultural significance*.

Where possible and culturally appropriate, removed significant fabric including contents, fixtures and objects, should be kept at the place.

#### Article 34. Resources

Adequate resources should be provided for *conservation*.

The best conservation often involves the least work and can be inexpensive.

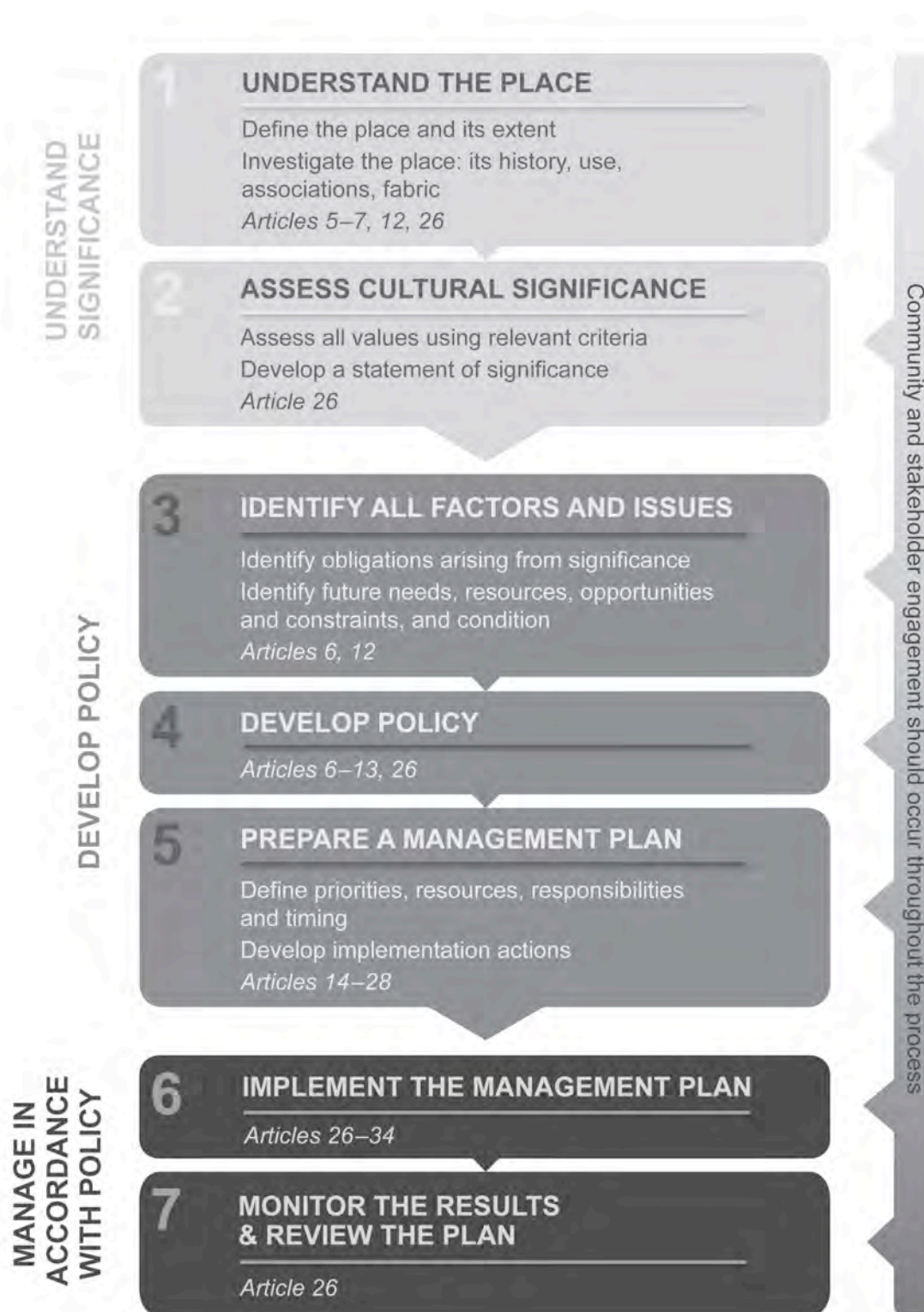
*Words in italics are defined in Article 1.*

# The Burra Charter Process

Steps in planning for and managing a place of cultural significance

The Burra Charter should be read as a whole.

Key articles relevant to each step are shown in the boxes. Article 6 summarises the Burra Charter Process.



## 8.5. Project Brief

## Request for Quotation – CT2328

### Project Specification

### *Fairfield Village Heritage Assessment*

#### 1. Scope

Darebin Council is looking to engage a suitably qualified consultant to assess selected properties in the Fairfield Village Activity Centre for heritage significance, prepare documentation as appropriate for the inclusion of places in the Heritage Overlay to the Darebin Planning Scheme, and provide advice on appropriate design outcomes for future development in these locations.

#### 2. Background

Darebin Council is in the process of preparing formal local policies to guide and manage development of land within the Commercial 1 Zone of the Fairfield Village Activity Centre.

Design guidelines are being produced to promote built form outcomes which balance state planning policy directives for greater development intensity in activity centres with the desire to retain the village feel of the centre and manage development impacts on residential precincts.

It is anticipated that draft Design Guidelines will be exhibited for community feedback by May 2016 and then finalised and formally adopted by Council by August 2017. The design guidelines are intended to be translated into planning scheme controls, with a planning scheme amendment process to commence in late 2017.

In preparing the guidelines to date, a number of buildings in the study area have been identified as providing a particularly valued contribution to the streetscape (refer to Study Area map below). The guidelines will provide measures which encourage the sensitive redevelopment of buildings with 'valued street facades' through the retention of these facades, appropriate setbacks of additional levels and use of respectful design elements and materials.

Council considers a more focussed assessment of these buildings is required to determine whether they also have local (or broader) heritage significance, individually and/or as part of a broader precinct. This will ensure future planning scheme controls appropriately recognise heritage value where it exists and can deliver appropriate development outcomes in Fairfield Village. Inclusion of justifiable properties within a Heritage Overlay will also provide enhanced protection from demolition and inappropriate alterations of identified valued street facades as advocated for in the design guidelines.

Assessment of buildings in Fairfield Village should draw from and consider previous heritage studies undertaken in the City of Darebin as appropriate. Relevant studies include:



## *Darebin Heritage Study 2007*

Prepared by Context Pty Ltd, the study mainly focused on the northern part of the municipality (in the former City of Preston) where no heritage studies had been undertaken, and also considered natural heritage and pre-European contact Koorie significance. The Study produced a number of key outputs.

### *Thematic Environmental History (August 2008)*

The Thematic History is the key piece of work arising from the 2007 Study for the purposes of identifying further places of historical significance in the City of Darebin.

The Thematic History provides an explanation of the historical themes and activities that have been important in shaping development throughout Darebin. The history is arranged according to themes so as to provide a context to assist with the identification of heritage places that illustrate the rich natural and cultural history of the municipality.

The assessment of properties in Fairfield Village is expected to draw from this document.

*Key findings and Recommendations (December 2008)* builds on the Thematic Environmental History study and provides detailed documentation and assessment of selected places, precincts, and post-contact archaeological places identified by Stage 1, with recommendations for Darebin Council on further work and other actions to manage heritage significant areas into the future.

*Review of 2000 Ward Study and Additional Stage 2 Place Assessments (May 2010)* – Stage 3 output - assesses the significance of additional places and includes a review of 50 places added to the Heritage Overlay as a result of the recommendations of a previous heritage study *Darebin Heritage Review 2000* (Andrew Ward and Associates).

*Historic Heritage Places 2011* – citations for 109 individual places and 18 added to the Heritage Overlay through Amendment C108 (part 1) to the Darebin Planning Scheme. These citations were translated to the Darebin Heritage online database (<http://vhd.heritage.vic.gov.au/vhd/darebin>)

Within the Fairfield Village Study area, the Fairfield Primary School and the St Pauls Anglican Church and Organ were added to the Heritage Overlay via Amendment C108 as a result of the above work.

## **Darebin Heritage Review 2000**

This Darebin Heritage Review contains heritage citation reports and statements of significance for most of the sites in Darebin which are affected by the Heritage Overlay (HO). The Study was undertaken post-amalgamation of the former Cities of Northcote and Preston and incorporates reviews of heritage places identified across former municipalities.

*Volume 1* – overall findings and recommendations, plus assessments / citations for identified heritage areas precincts.

*Volumes 2&3* – citations for individual places

The Fairfield Railway Station, Fairfield Signal Box and the Fairfield Post Office, all of which are within the Fairfield Village study area, were identified for inclusion in the Heritage Overlay through this Review as individual places of significance.

### 3. Study Area

The area for investigation is generally defined by the Commercial 1 zoned land, comprising the main retail centre along Station Street north of the railway line and sections of Wingrove Street and Railway Place north and south of the railway line.

Heritage assessment is required for properties marked on the map extracted from the draft Design Guidelines work as providing 'valued street facades'. The assessment may extend to include other properties within the investigation areas as deemed appropriate on visual inspection.

Existing Heritage Overlays within and adjoining the Study Area are also indicated for clarity.



The following individual and clusters of properties are of particular interest:

- 1-5 Railway Place
- 11-15 Railway Place and 46 Gillies Street
- 17-27 Railway Place
- 75 & 81 Station Street
- 282- 284 Wingrove Street (in addition consider no. 278 adjacent)
- 268 Wingrove Street (in addition also consider no. 266 adjacent)

#### 4. Project Tasks and Methodology

The tasks and methodology represent a proposed outline by Council. These can be refined by the consultant if deemed beneficial to the process and outcomes to be achieved, however any variations must be specified and priced separately. A final outline, including timelines and milestone deadlines are to be confirmed at the inception meeting.

##### Stage 1: Preliminary Assessment and Nomination

- a) Review the City of Darebin Heritage Study *Thematic Environmental History (August 2008)*.
- b) Familiarise with earlier heritage studies and existing heritage overlay supporting documentation as relevant to the study area.
- c) Undertake and document a preliminary assessment of the Study Area to identify and nominate places of significance. The preliminary assessment is to focus on identified properties in this brief but also including other properties as deemed appropriate on visual inspection, with consideration against the various heritage criteria, especially relating to the Thematic History and the quality/integrity of the item.
- d) Submit the preliminary assessment for the consideration of the Project Manager who will select nominated places to be progressed to Stage 2.
- e) Review draft Fairfield Village Design Guidelines (*will be provided by Council*) in relation to properties recommended for inclusion in the heritage overlay and provide preliminary recommendations on design approaches and standards for redevelopment activity on these properties.

##### Stage 2: Detailed Assessment and Documentation (if required)

- f) Investigate, research and document selected places to sufficient standard to enable Council to proceed with planning scheme amendments to apply the heritage overlay. See Deliverables below for format of documentation required.
- g) Confirm recommendations for statutory protection via the Darebin Planning Scheme and draft appropriate controls, or modifications to existing controls, for inclusion within the Darebin Planning Scheme.

#### 5. Deliverables

The project has the following deliverables:

##### End of Stage 1

- a) Written report summarising preliminary assessment findings. The report should detail nominated places via identifying photo and brief justification, for consideration by the project manager.
- b) Preliminary written advice (in the form of a memorandum or short report) recommending design approaches and standards that should be included in the Fairfield Village Design Guidelines (*an early draft will be provided by Council to assist with this*) to guide appropriate and sensitive redevelopment of nominated heritage places.

##### End of Stage 2

- c) Finalised written advice (in the form of a memorandum or short report – can be an update to Stage 1 output) confirming recommended design approaches and standards to be included in the Fairfield Village Design Guidelines and any subsequent planning scheme controls to guide appropriate and sensitive redevelopment of heritage places.
- d) Documentation sufficient to enable Council to proceed with planning scheme amendments for recommended places, as follows:

- Citations for each selected place which:
  - ✓ are clear, understandable and logical, especially in defining significance and relevance to thematic history, and to help inform designs and works proposed in development applications.
  - ✓ where a proposed heritage overlay is comprised of more than one key element, e.g. a precinct of buildings, nominate wherever possible each main component as 'significant', 'contributory' or 'non-contributory' to assist interpretation and assessment of development applications.
  - ✓ provide owners, designers and planners with guidance about relative significance of elements, and general guidelines / design principles that would be applicable for total and partial demolition, alterations, additions and redevelopment.
- At least one clear, optimally exposed photo of each item that enables easy identification, and includes where reasonably possible, all significant and contributory elements, especially where the proposed heritage overlay is a precinct for a complex comprised of numerous items.
- Clear maps of proposed heritage overlay boundaries, siting and locations of sufficient standard, and on an appropriate scaled cartographic base to enable public exhibition, reference at any panel hearing and easy translation into planning scheme mapping.
- Draft heritage overlay Schedules and any other recommended statutory controls, including modifications to existing controls, as required for inclusion in the Darebin Planning Scheme.
- Entries for all recommended places into the HERMES electronic database provided by Heritage Victoria.

All documentation and material is to be produced in formats as agreed with Council (e.g. reports in PDF, word, InDesign or similar; maps separately in PDF, jpg, MapInfo or similar; data in tables & excel files).

## 6. Important Dates

Request for Quotation closes: 6 December 2016 at 2pm AEST

The new Contract will be in place by 20 December 2016.

This project is to be completed within anticipated delivery timeframes already set for the Fairfield Village Design Guidelines and commencement of a planning scheme amendment process. As there is limited flexibility in changing these dates, further variation will be by agreement with Council.

Project Stage	Tasks/Milestones	Due Date	Council
<b>Stage 1</b>	Project Inception	20 December 2016	Meeting to be scheduled
	Draft Stage 1 Deliverables (items a-b)	24 February 2017	Officers to review and provide feedback by COB 10 March 2017
	Final Stage 1 Deliverables (items a-b)	24 March 2017	
<b>Stage 2</b>	Finalised design advice (item c)	28 April 2017 (negotiable)	
	Draft detailed Assessments and Citations (item d)	12 May 2017	Officers to review and provide feedback by start of June 2017
	Final Deliverables	30 June 2017	

## **8.6. Citation - 278 Wingrove Street Fairfield**



<b>Locality:</b>	<b>FAIRFIELD</b>
<b>Place address:</b>	<b>278 WINGROVE STREET</b>
<b>Citation date</b>	2017
<b>Place type (when built):</b>	Residence, with addition of 'lodge' medical practice at the front.
<b>Recommended heritage protection:</b>	Local government level Vic Heritage Register: No Heritage Inventory (Archaeological): No Local Planning Scheme: Yes
<b>Place name:</b>	<b>Fairfield Dermatology Clinic</b>



<b>Architectural Style:</b>	Inter-War Spanish Mission
<b>Designer / Architect:</b>	[Not Known]
<b>Construction Date and Builder:</b>	c1928 (front), c1907 (rear): Possibly built by contractor Alexander E V Oliver.

## Statement of Significance

This statement of significance is based on the history, description and comparative analysis in this citation. The Criteria, A, B, C, D, E, F, G, H is the Heritage Council Criteria for assessing cultural heritage significance (HERCON). Heritage values of historic, social, aesthetic and scientific significance are based on the Burra Charter definitions. Level of Significance, Local, State, National, is in accordance with the level of Government legislation.

### *What is significant?*

The premises at 278 Wingrove St, Fairfield, is significant. The original form, materials and details of the building and fence, as constructed in c1928 are significant. Later buildings and alterations to the earlier building are not significant.

### *How is it significant?*

The premises at 278 Wingrove St, Fairfield, is locally significant for its historical, social and aesthetic values.

### *What is significant?*

The premises is **historically significant at a local level** for its original purpose as a lodge medical practice and doctor's residence, begun and run by Dr Leslie Currie until from 1925 until the late 1950s in advance of his retirement in 1961. The new section constructed in 1928 in front of the original 1907 residence, was specifically designed with two front entries, one for the surgery, and one for the residence. It is also historically significant at a local level as an uncommon surviving example of a small scale lodge medical practice in Fairfield and Darebin City. The working class nature of the suburb of Fairfield contemporary to the commencement and development of the Wingrove Street medical practice is demonstrated by the decision to locate a Lodge medical practice opposite the railway station. Lodge medical practices are an important feature in the history of the provision of medical services to the Australian community and parts of the present day system of health care in Australia evolved from this model of medical practice. (HERCON criteria A and B)

The premises at 278 Wingrove Street is **socially significant at a local level** due to its association with the Wingrove Medical Practice, which has served the Fairfield community continuously for at least 92 years, from 1925 to 1963 at 278 Wingrove St ( Dr Leslie Currie) and from 1963 until today ( 2017) at 270 Wingrove St ( purpose built by Dr Robert Currie, son of Dr Leslie Currie, and Dr Madden. It is also of local significance due to the direct relationship of community members to the building as the site for births, deaths, care and service for 38 years before the Wingrove Street medical practice relocated to its present site. (HERCON criteria G and H).

The medical premises is **aesthetically significant at a local level** for its unobstructed view from the railway station, intact representative Inter-War Spanish Mission style architecture which presents primarily as a single storey domestic scaled house, set close to the street boundary, with a low front fence and garden, however, the surgery entrance is denoted with a prominent corner 'tower'. The Spanish Mission style building has distinctive features including rendered walls above sill height, clinker brick dado, round arched loggia, gable end, distinctive surgery entry parapet and gabled chimney top with Spanish tile capping, and ledge moulding over a second entry. (HERCON criterion E).

## Statutory Recommendations

This place is recommended for inclusion in the Schedule to the Heritage Overlay of the City of Darebin Planning Scheme as part of the Fairfield Village Heritage Precinct, with the boundaries as shown on the map.

<b>External Paint Controls</b>	Yes
<b>Internal Alteration Controls</b>	Yes: front portico/loggia only.
<b>Tree Controls</b>	No
<b>Outbuildings or fences which are not exempt under Clause 43.01-3</b>	Yes: front fence only
<b>Prohibited Uses May Be Permitted</b>	No
<b>Incorporated Plan</b>	Yes
<b>Aboriginal Heritage Place</b>	Not assessed

## History

### Thematic context

This place is associated with the following themes identified in the City of Darebin Heritage Study Volume 1 Draft Thematic Environmental History Stage 2 Report (2008)

- 4 Developing Darebin's Economies
  - 4.6 Providing local shopping and services
- 5 Building Suburban Darebin
  - 5.3 Developing the suburban ideal and Twentieth century recovery
- 7. Community and Culture
  - 7.4 Providing Health and Welfare Services

### Place history

Wingrove Street was originally named Railway Place North and 278 Wingrove Street was rated as 12 Railway Place North in the Heidelberg Shire Rate Books until 1930 when this entry was crossed out and changed to the present address. (Heidelberg Shire Rate Books, Fairfield Riding 1928-1935) For the purpose of this history the name Wingrove Street has been used throughout but where information is from before 1930 the street would have been referred to as Railway Place North.

278 Wingrove Street Fairfield is part of land known as portion 113 Parish of Jika Jika, County Bourke granted to A Walker on 10 June 1840. Portion 113 appears to have remained unoccupied and undeveloped until it came into the possession of Charles Henry James and Percy Dobson in 1883. James and Dobson owned the adjacent portion 114 and undertook multiple subdivisions of both properties between 1883 and 1885 in order to create the Fairfield Park Estate. (Certificate of Title Volume 1515, folio 976) (Figure H1)

In 1886 Dr Walter Balls Headley purchased several allotments in the Fairfield Park Estate including lot 51 of which 278 Wingrove Street is a part (Volume 1820 Folio 958 Certificate of Title). The land remained undeveloped during the depression of the late nineteenth century but as the economy was recovering in 1906 contractor Alexander E V Oliver purchased lot 51 and immediately subdivided and sold the land as four individual allotments. (Certificate of Title, Volume 3137 Folio 627378) (Figure H2)

By 1907 each portion of lot 51 included a house occupied by a tradesman or labourer (Heidelberg Shire Rate Records, Fairfield Riding 1907-1908). The house at 278 Wingrove Street was called 'St Leonards'. (Figure H3) Dr Leslie Currie and Mrs Ellen Currie first leased lot 51 in 1925 (Heidelberg Rate Records, Fairfield Riding 1925) and "...set up a 'lodge' medical practice in the front rooms of the house." (Royal Australian College of General Practitioners (RACGP), 2010)

Following their purchase of the property in 1928 (Certificate of Title Volume 3193 Folio 638597) Dr and Mrs Currie appear to have modified the existing building (as the M-shaped roof form of the rear section of the present building, which is visible in aerial photos, is consistent with that of a Victorian/Federation era building (although physical inspection may reveal that the existing building was demolished), and built the present Inter-War Spanish Mission style section of the dwelling in front of the older house intending the whole to serve as both a home and medical practice.

The decision to set up a medical practice in Wingrove Street opposite Fairfield Park Railway Station was strategic. (RACGP, 2010) The surface of what became Wingrove Street had been 'made' in 1913 (The Age, 10 February 1913, p.16). and an article in the Weekly Times in 1921 described the rapid growth in Fairfield as:

"Houses, shops and public buildings have sprung up, roads have been made, the population constantly increasing, and it is rapidly becoming a most popular suburb.... Station Street and Railway Place are fast becoming busy thoroughfares, and have many shops of the modern type." (Weekly Times Melbourne Sat 25 June 1921 page 48)

In 1921 the Heidelberg railway line was electrified reducing commuting times to Melbourne (Weekly Times, 25 June 1921, page 48, Trove). Around the same time swampy areas of Fairfield were drained (Darebin Heritage, Fairfield) and employment opportunities in the area grew due to the development of the Australian Paper Mill (Darebin Heritage, Amcor, The Australian Paper Manufacturing Company) and growth of the Queens Memorial Hospital. (Darebin Heritage, Fairfield Infectious Diseases Hospital) The improved amenity and opportunity of Fairfield compared favourably against the overcrowded conditions of suburbs closer to the city and Fairfield grew accordingly. (Darebin Heritage, Fairfield)

According to Dr Robert Currie who grew up at 278 Wingrove Street, Dr and Mrs Currie and their five children (of which he was one) "...lived in the back of the building (and were) eventually housed in an increasing number of bungalows at the rear of the building" (RACGP, 2010).

Describing the medical practice Dr Robert Currie detailed that;

"Lodge patients paid their dues to a 'friendly society', and the doctor was paid by the number of patients on his books. There were no other fees paid, but grateful patients would sometimes bring a gift of potatoes, eggs or other homegrown produce. There were the occasional private patients who could afford the half-guinea fee. Such patients were ushered into the front lounge room and seen on a priority basis.

There was no appointment system and patients would wait their turn on the rather firm benches in the waiting room, sometimes entertained by the antics of the psychotic next-door neighbour, whose language was frequently loud and fruity!" (RACGP, 2010)

Dr Leslie Currie operated a solo medical practice during the war years after which time he was joined by another practitioner before handing the practice over to Dr Kenneth Madden in the 1950s. (RACGP, 2010) Although no longer the principal, Dr Leslie Currie continued to work at the practice until his retirement in 1962. (RACGP, 2010)

Dr Robert Currie describes how during the 1950s "...more areas of the 278 Wingrove Street building were taken over as consulting rooms as my siblings left home and an additional branch surgery was established in Darebin Road, Thornbury, approximately 3 kilometres away from the main rooms." (RACGP, 2010)

When Ellen Maude Currie died in 1961 278 Wingrove Street was still her place of residence (Probate and Administrative Files, Ellen Maude Currie) as well as a medical practice. Following Dr Leslie Currie's retirement not long after his wife's death their son Dr Robert Currie took over the practice.

278 Wingrove Street ceased to operate as a medical practice in 1963 when Dr Robert Currie acquired 270 Wingrove Street from his mother's deceased estate (RACGP, 2010, Probate and Administration Files, Ellen Maude Currie). A 'purpose-built surgery' was erected at the new premises, the original branch surgery closed and 278 Wingrove Street was sold. (RACGP 2010)

Mr and Mrs Sterry purchased 278 Wingrove Street in 1963. (Certificate of Title Volume 8453 Folio 805) During this time the property appears to have been operated as flats occupied by the Kurbanovs from 1964 until around 1970 (Melbourne Sands & McDougall Directory 1964 - 1974) at which time the Kurbanovs purchased the property. 278 Wingrove Street has changed hands several times since 1970 and has operated as a dermatology clinic since at least 2010. (RACGP 2010, Certificate of Title Volume 8453 Folio 805)

278 Wingrove Street was used from around 1906 to 1924 to meet the residential needs of the working class people of Fairfield Park. The front section of the present dwelling at 278 Wingrove Street appears to have been constructed during 1928/1929 to serve as a residence whilst addressing the medical needs of the Fairfield Park Estate community. Further modifications have since been made to the property as the needs of the residents and community grew and changed.

## Lodge Medical Practices

Lodge medical practices or 'friendly societies' were "...established in Australia during the late nineteenth century and by 1900 about one quarter of the population was covered for medical expenses by contracts drawn up between the societies and individual doctors." (Thame, 1974, p.295)

Lodge practices were intended to cover the gap between people who qualified for free medical care at that time and those who could afford to pay. (General Practitioners of Australia website, 1920s)

Lodge members paid an annual fee for which the lodge or society then paid their medical treatment expenses and/or funeral benefits when required. (General Practitioners of Australia website, 1920s)

As well as being integral to the welfare of a significant percentage of the population, lodges represented both financial security and financial constraint to doctors depending on the location and individual circumstances of their medical practice. (General Practitioners of Australia website, 1920s)

Regional lodge practices where the patients were likely to represent a range of economic circumstances were more lucrative than some suburban practices, particularly in predominantly working class suburbs, where there were few paying customers able to supplement the modest remuneration provided by the lodge contracts.

Despite the inadequate income generated by some lodge practices during the interwar years 40-60% of doctors "... in the Eastern States and South Australia...relied on lodge payments for a substantial part of their income." (Gillespie, 1988, p.35)

Twentieth century general medical practice was characterised by decades of at times open dissent between government, the British Medical Association (BMA) (Australia) and the friendly societies. (Gillespie, 1988, pp. 34-36 & p.46) The principal concerns of each group were who determined the cost of medical services, how those services were paid for and who decided who paid for those services. (Gillespie, 1988, pp. 34-36) The Friendly Societies were opposed to the removal of lodge contracts which were seen by the BMA as disempowering and disadvantageous to doctors. The government was keen to provide a system that equated to the least financial outlay in return for the most medical coverage for the most people and the BMA lobbied for autonomy from lodge contracts to be replaced by some kind of support for those unable to afford medical fees. Opinions about the



nature of that support varied considerably in the medical profession and the position of the BMA at various times changed accordingly. (Gillespie, 1988, pp. 44-46)

Eventually the BMA prevailed and in 1951 they succeeded in having the contract based lodge practice abolished. (Thame, 1974, p.338) A national subsidised fee-for-service health service was introduced instead which provided 3 tiered scaled means tested assistance with the bulk of patients in the middle category following a model that was "...essentially an elaboration of the lodge system with the substitution of the Government for the friendly societies." (Byrne C, 1943 p.13 in Gillespie, 1988, p.46)

Friendly Societies or Lodges known to have been located in Fairfield that Dr Currie may have been contracted to included the

Fairfield Masonic Lodge established in 1919 (Fairfield Lodge No 254) originally meeting at the corner of Gillies Street and Railway Place North (?),

A.N.A. Friendly Society established in 1920 (Darebin Heritage George Henwood)

A.W.A (Darebin Heritage George Henwood)

M.U.I.I.O.F. (Darebin Heritage George Henwood)

Independent order of Oddfellows (Parliamentary Report Victoria, Report of the Friendly Societies for the Year 1904)

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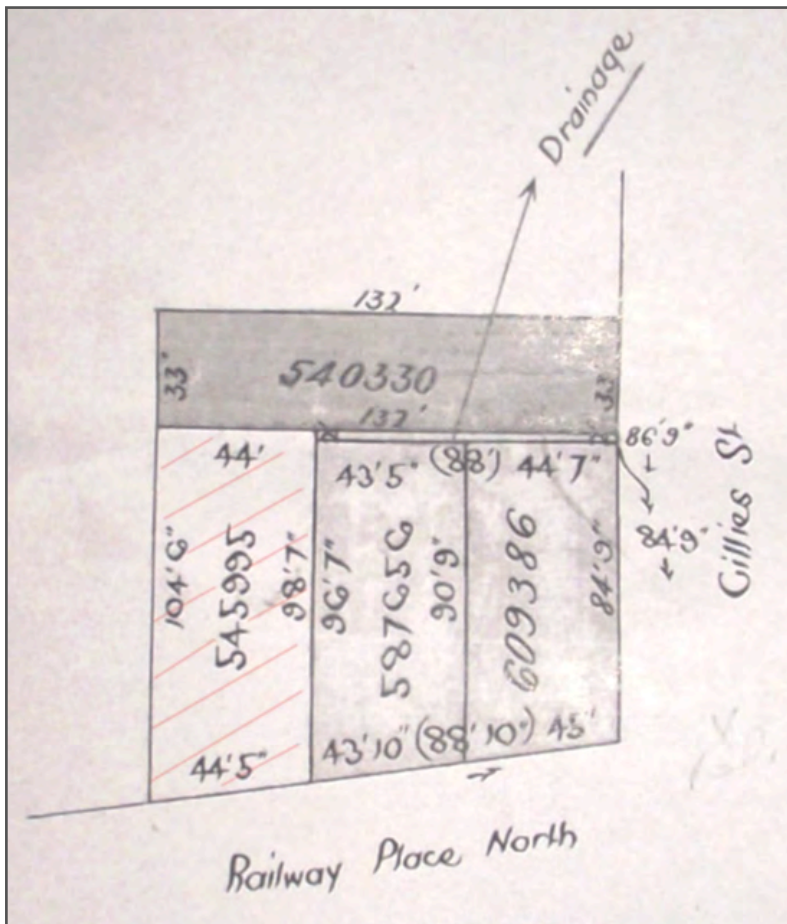
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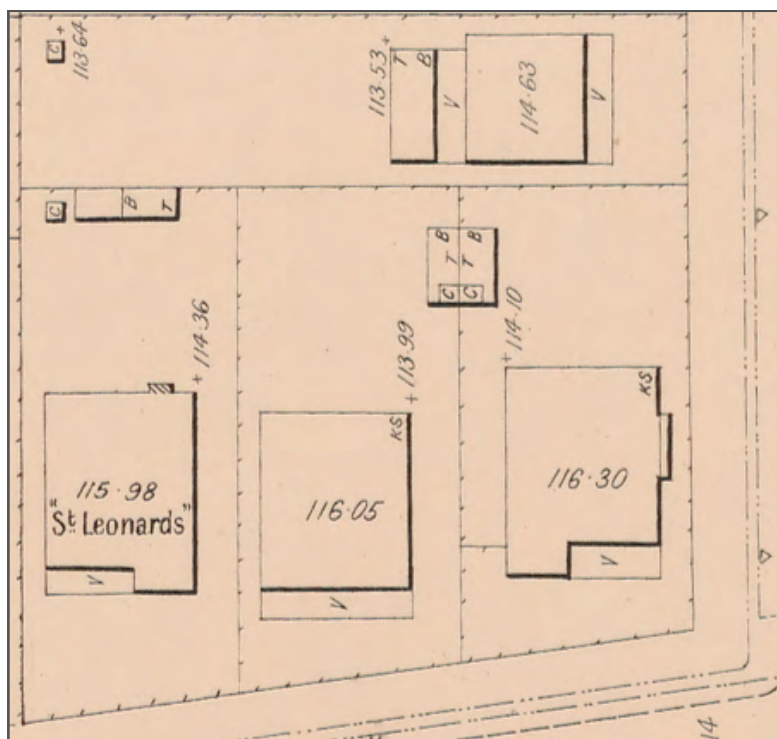
## History - Illustrations



**Figure H1.** Subdivision Plan for Rathmines Street and Railway Place North (Wingrove Street) Fairfield Park Estate. The land incorporating 278 Wingrove Street is hatched in red. Source: Subdivision Plan 547 Certificate of Title Volume 1820, Folio 958.



**Figure H2** Subdivision plan of lot 51, corner of Gillies Street and Railway Place North (Wingrove Street). The land which became 278 Wingrove Street is hatched in red. Source: Subdivision Plan dated 29 January 1907 Certificate of Title Volume 3137 Folio 627378



**Figure H3** Map showing first house at 278 Wingrove Street known as 'St Leonards', 1912. **Source:** Melbourne Metropolitan Board of Works Plan 2473, State Library Victoria)

## Description

This section describes the place in 2017. Refer to the Place History for additional important details describing historical changes in the physical fabric.

The c1928 place is an intact representative Inter-War Spanish Mission style building which presents primarily as a single storey domestic scaled house, set close to the street boundary, with a low front fence and garden, however, the surgery entrance is denoted with a prominent corner 'tower'. The Spanish Mission style building has distinctive features including rendered walls above sill height, clinker brick dado, round arched loggia, gable end, distinctive surgery entry parapet and gabled chimney top with Spanish tile capping, and ledge moulding over the residence entry. It is clearly visible from the Railway Station and railway reserve opposite. The c1928 building is in very good condition with high integrity, although the 'garage' at the west end of the front elevation has been altered with the insertion of a 'roller door'. The earlier c1907 building at the rear, is not visible from the street and not significant.

## Descriptions - Illustrations.

All photos taken in 2016-17 by Heritage Intelligence Pty Ltd



**Figure D1.** View showing the Spanish Mission style features of rendered walls above sill height, clinker brick dado, round arched loggia, Spanish tiles, gable end, distinctive surgery entry parapet, gabled chimney top and ledge moulding over the residence entry.





**Figure D2.** Distinctive parapeted entry porch with Spanish tiles, and gabled chimney top with round arched openings and Spanish tiles.



**Figure D3.** Elaborate twisted decorated column with acanthus leaf capital, rendered walls above the dado of clinker bricks, rectangular timber windows with nine lights over one, and timber doors and timber lined ceiling.



**Figure D4. The low fence has been designed to match materials and features of the building, showing rendered panels between clinker brick plinth and angled clinker brick headers, supported by clinker brick piers.**

## Comparative analysis


The building at 278 Wingrove Street is a commercial premises that appears to be comprised of an original c1907 residential dwelling called 'St Leonards' at the rear and a c1928 lodge practice surgery extension, in the Inter-War Spanish Mission style, fronting the street. The building appears to be used for commercial purposes only at present.

However as it historically functioned simultaneously as a residential, service and commercial operation this comparative study considers examples of both comparable residential building design and scale and residential medical practices operating around the same time.

The original building at 278 Wingrove Street appears to be a Federation style weatherboard building and was one of four similar buildings erected around 1907 when 278 Wingrove Street was created from a suburban subdivision. (Certificate of Title Volume 3137 Folio 627378) The three other original buildings from this subdivision do not appear to have survived.

The extension to the front of St Leonards is built in the Inter-War Spanish Mission style. A search of the Victorian Heritage Database (VHD) indicates that the only Inter-War Spanish Mission style place in the City of Darebin which is of State Significance is the Ernest Jones Hall which forms part of the former Mont Park Hospital. (H1872, HO59) (Table 1) There appear to be no other examples of Spanish Mission style buildings in Fairfield recognised in the VHD.

Table 1

<p>Former Ernest Jones Hall &amp; Chapel Former Mont Park Hospital</p> <p>Ernest Jones Drive and Springthorpe Boulevard and Cherry Street Macleod Darebin City</p>		<p>Former Mont Park Hospital Victorian Heritage Register H1872 Darebin City H059 Image Source Adam Dimech, 2010 Flickr viewed 14/4/2017 &lt;<a href="https://www.flickr.com/photos/adonline/4472982880">https://www.flickr.com/photos/adonline/4472982880</a>&gt; (reproduced without permission)</p>
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The limited recognition of Spanish Mission style buildings in Darebin City and particularly Fairfield may result from a scarcity of examples in what was a working class area. The City of Darebin Heritage Study describes 'romantic' inter-war house styles including Mediterranean as being '...more common in middle-class suburbs of Melbourne, but relatively rare in working-class...' suburbs. (Context, City of Darebin Heritage Study, 2011, p. 540)

A broad search of the VHD illustrates that more than half of the Inter-War Spanish Mission style listings are for substantial commercial, religious, public or government buildings. There are at least 20 examples of Inter-War Spanish Mission style residential properties in the VHD and at least eight streets or precincts in which are identified Inter-War Spanish Mission style properties or interwar buildings that include Inter-War Spanish Mission style features. There are no Inter-War Spanish Mission style residences of State significance recorded on the VHD.






Spanish Mission style residences listed on the VHD include single (Table 2) and two storey dwellings, apartment complexes, residential estates and subdivisions which include elements of Inter-War Spanish Mission style as part of a complex of interwar period buildings. The registered buildings are generally substantial in size, whereas modest sized examples of Inter-War Spanish Mission style such as 101 and 120 Melville Road Pascoe Vale South (Table 2) are uncommon. The more substantial Spanish Mission style properties generally exhibit more design style features than the smaller examples. 278 Wingrove Street is both modest in size and typical of the Inter-War Spanish Mission style.

Table 2

Examples of Single Storey Dwellings in Spanish Mission Style registered on the VHD

Property	Image	Heritage Citation Detail
<p>Raellein 26 Melcombe Road Ivanhoe</p>		<p>House H044 Banyule City</p> <p>Image Source VHD Report</p>



<p>86 Aphrasia Street Newtown</p>		<p>House H01933 Greater Geelong City Image Source VHD Report &lt;<a href="http://vhd.heritagecouncil.vic.gov.au/places/24377">http://vhd.heritagecouncil.vic.gov.au/places/24377</a>&gt;</p>
<p>56 Keam Street Ivanhoe East (formerly 356 Lower Heidelberg Road)</p>		<p>House H040 Banyule City  Image Source VHD Report</p>
<p>29 Studley Road Ivanhoe</p>		<p>House H0153 Banyule City Image Source VHD Report 14/4/2017 &lt;<a href="http://vhd.heritagecouncil.vic.gov.au/search?kw=29+studley+road+ivanhoe&amp;aut_off=1">http://vhd.heritagecouncil.vic.gov.au/search?kw=29+studley+road+ivanhoe&amp;aut_off=1</a>&gt;</p>
<p>Aloha 1 Stanley Street Essendon</p>		<p>House H07 Moonee Valley City  Image Source VHD Report &lt;&gt;</p>
<p>120 Melville Road Pascoe Vale South</p>		<p>Melville Road Precinct H0110 Moreland City  Image Source Google Maps 14/4/2017 &lt;<a href="https://www.googlemaps.com.au">https://www.googlemaps.com.au</a>&gt;</p>

101 Melville Road Pascoe Vale South		Melville Road Precinct HO110 Moreland City  Image Source Google Maps 14/4/2017 < <a href="https://www.googlemaps.com.au">https://www.googlemaps.com.au</a> >
11 Allaville Avenue Glen Iris		Gardiner Park Estate Precinct HO387 Stonnington City  Image Source VHD Report 14/4/2017 < <a href="http://vhd.heritagecouncil.vic.gov.au/places/118043">http://vhd.heritagecouncil.vic.gov.au/places/118043</a> >
293 Pakington Street Newtown		Residence HO1947 Greater Geelong City  Image Source VHD Report 14/4/2017 < <a href="http://vhd.heritagecouncil.vic.gov.au/places/25672">http://vhd.heritagecouncil.vic.gov.au/places/25672</a> >

The City of Darebin Heritage Study identified two surviving inter war year doctors surgeries, both incorporate houses and are located in Preston and Reservoir. Both of these properties are substantial double storey buildings built in the Georgian Revival Style that was also popular in the interwar period. (City of Darebin Heritage Study, 2011, pps 534-543 and 712-717).

There are several doctors surgeries on the VHD all of which include a dwelling. An extensive search of the VHD indicates that only five of the registered 'house and surgery' buildings are constructed after 1915. Although the operation of a lodge practice from these premises might be implied none of the entries are specifically identified as being lodge medical practices.

Compared to nearly all Inter-War Spanish Mission style properties listed on the VHD 278 Wingrove Street is unusual in that it appears to have been erected as an addition to an existing property. An exception to this is the Williams House of Princes Hill (Yarra City, HO329) (Table 3) the present appearance of which resulted from the Spanish Mission style renovation of an 1880s house. (VHD Report Williams House, 2014). **Table 3**

Williams House 218 Macpherson Street Princes Hill Yarra City		Princes Hill Heritage Overlay Area HO329 Yarra City  Image Source VHD Report 14/04/2017 < <a href="http://vhd.heritagecouncil.vic.gov.au/places/103118">http://vhd.heritagecouncil.vic.gov.au/places/103118</a> >
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Whilst a lodge medical practice provides a community service it is also a commercial entity. Examples of commercial premises built in the Inter-War Spanish Mission style on the VHD are generally more substantial buildings than 278 Wingrove Street.

## Management Guidelines

Whilst landowners are not obliged to undertake restoration works, these guidelines provide recommendations to facilitate the retention and enhancement of the culturally significant place, its fabric and its setting, when restoration works or alterations to the building are proposed. They also identify issues particular to the place and provide further detailed advice where relevant.

That part of the property that is visible from Wingrove St, which is primarily the c1928 extension, is significant. The map below illustrates the position of the c1907 house 'St Leonards' relative to the 2016 aerial view of the property showing the c1928 tiled roof of the Inter-War Spanish Mission extension to the south. The red rectangle marks the approximate location of the property boundary since the 1906 subdivision. The inset diagram shows a polygon shaded in blue which illustrates the preferred location for additions and new development.

### 1. Additions and new buildings

- 1.1. Retain clear views of the front elevation back to and including the chimney.
- 1.2. Retain the visual connection to and from the Railway Station.
- 1.3. The blue shaded area in Figure xx is the preferred location for additions and new development.

### 2. Reconstruction and Restoration

- 2.1. Never sand, water or soda blast the building as this will permanently damage the bricks, mortar and render.
- 2.2. Do not paint or seal the brick work.
- 2.3. Replace the missing Spanish tiles on the chimney.

### 3. Signage

- 3.1. Ensure all signage is designed to fit around the significant architectural design features, not over them. The existing signage is a good example of the size and location of appropriate signage.



## 8.7. Precinct - Illustrated Schedule of Places

## APPENDIX 7

### Fairfield Village Heritage Precinct

### SCHEDULE OF PLACES

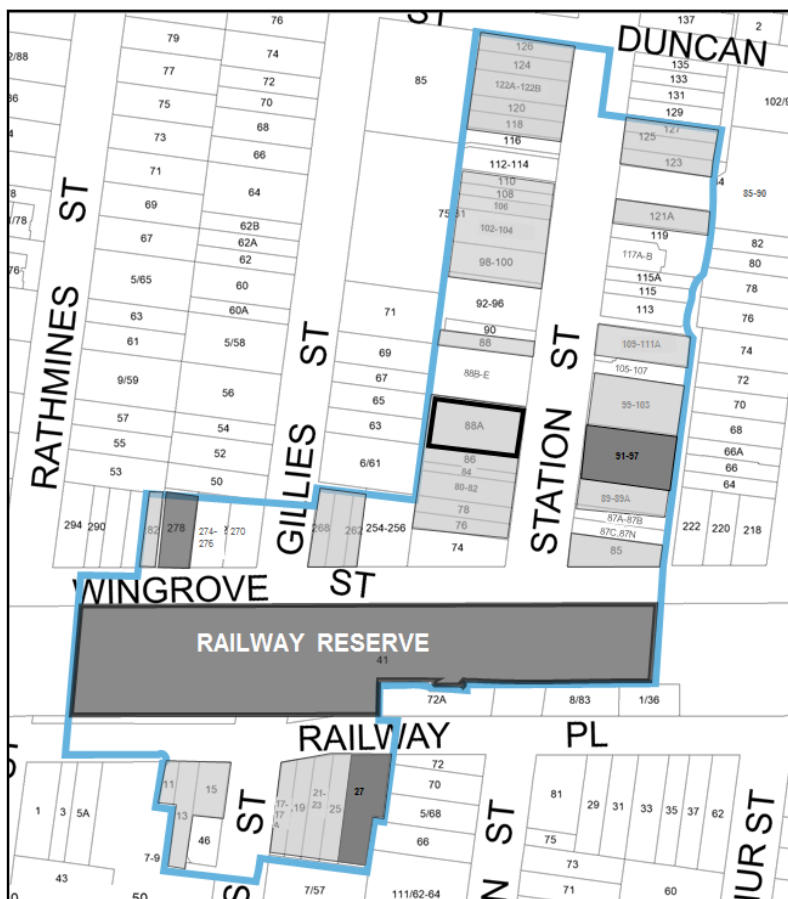
## Fairfield Village Heritage Assessment






Prepared for Project Manager

Gilda Di Vincenzo

City of Darebin




30 June 2017





KEY	
Fairfield Village Heritage Precinct.	
	Proposed Heritage Overlay Precinct boundary.
	Location of existing Heritage Overlay within the proposed precinct. HO190 church.
	Location of <i>Significant</i> heritage places within the proposed precinct.
	Location of <i>Contributory</i> heritage places within the proposed precinct.
	Location of not significant/not contributory places.

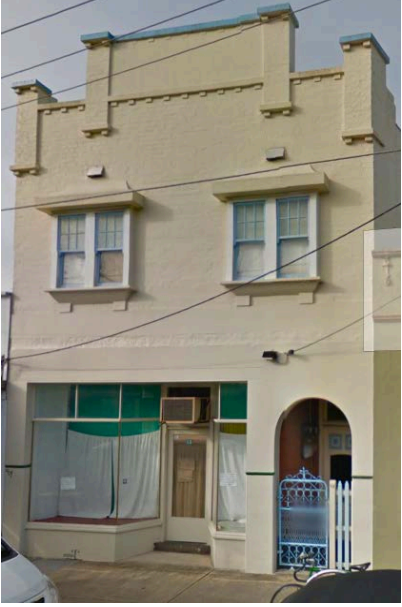


## DEFINITIONS




Heritage Place	Significant	A <i>Significant</i> place is a single heritage place that has cultural heritage significance which may be independent of its context. These places may also contribute to the significance of a heritage precinct. <i>Significant</i> places within a heritage precinct will not usually have a separate Statement of Significance.
	Contributory	A <i>Contributory</i> place contributes to the significance of a heritage precinct, but would not be significant on their own.
	Significant feature	A <i>Significant feature</i> is any feature (building, tree, structure etc.) that the relevant <i>Fairfield Village Heritage Assessment</i> (2017) identifies as contributing to the significance of a heritage place or precinct. <i>Significant features</i> are described in the statement of significance for the place or precinct and in the Schedule in the <i>Fairfield Village Heritage Assessment</i> (2017).
Non Heritage Place	Non-contributory or Not Significant	<i>Non-contributory or Not Significant</i> places do not contribute to the significance of a heritage precinct. In some instances, a <i>Significant</i> place may be considered <i>Non-contributory or Not Significant</i> within a precinct. For example, an important Modernist house within a Victorian era precinct.
Maintenance		<i>Maintenance</i> means the continuous protective care of a <i>place</i> , and its <i>setting</i> . Maintenance is to be distinguished from repair which involves <i>restoration or reconstruction</i> .



Street No.	<b>Photograph</b> All photos are by Heritage Intelligence Pty Ltd, unless noted otherwise.	<b>Heritage Significance</b> As defined on page one of this Schedule.
46	<b>GILLIES STREET</b> 	<p><i>Not Significant</i></p> <p>Modern interpretation of historic forms, elements and materials.</p>
11	<b>RAILWAY PLACE</b>  <p>Source: Google Street View.</p>	<p><i>Contributory</i></p> <p><i>Significant Features</i>  Front elevation including</p> <ul style="list-style-type: none"> <li>• Parapet</li> <li>• Ingo - recessed entry.</li> <li>• Timber window frames</li> <li>• Stallboard</li> <li>• (Note: the verandah is missing.)</li> </ul>
13		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Front elevation including</p> <ul style="list-style-type: none"> <li>• Parapet</li> <li>• First floor decorative pilasters and classical columns</li> <li>• Ingo - recessed entries.</li> <li>• Window frames</li> <li>• Stallboards</li> <li>• Verandah</li> </ul> <p>•Note: Some wall decoration and the original first floor windows are altered or missing.</p>



Street No.	<b>Photograph</b> All photos are by Heritage Intelligence Pty Ltd, unless noted otherwise.	<b>Heritage Significance</b> As defined on page one of this Schedule.
15		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Front elevations facing Gillies St and Railway Place, including</p> <ul style="list-style-type: none"> <li>• Roof form and materials</li> <li>• Face brick walls (now overpainted)</li> <li>• Splayed corner</li> <li>• First floor decorative plaque "STATION BUILDINGS A.D.1922"</li> <li>• Ingo - recessed entries.</li> <li>• Ground floor metal window frames with highlights.</li> <li>• First floor double hung timber windows.</li> <li>• Stallboards with green glazed tiles</li> <li>• Piers with green and checkerboard strips of glazed tiles.</li> <li>• Verandah</li> </ul> <p>•Note: The brickwork has been painted and the quad spouting has been replaced with a square spouting.</p>
17-17A	 <p>Source: Google Street View</p>	<p><i>Contributory</i></p> <p><i>Significant Features</i>  Front elevation including</p> <ul style="list-style-type: none"> <li>• Parapet with curved central skyline, semicircle cut outs, pilasters at each end, rendered with flat capping,</li> <li>• Ingo - recessed entry.</li> <li>• Metal window frames with highlights</li> <li>• Stallboards</li> </ul> <p>•Note: The posted verandah, visible in an early photo, is missing.</p> <p>New first floor extension is not significant.</p>

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19		<p><i>Contributory</i></p> <p><i>Significant Features</i></p> <p>Front elevation including</p> <ul style="list-style-type: none"> <li>• Parapet</li> <li>• First floor brick walls with decorative pilasters, dentils, flat capping, concave window hoods over the first floor windows with brackets.</li> <li>• First floor projecting double hung window frames with geometric lead lighting.</li> <li>• Ingo - recessed entries.</li> <li>• Ground floor metal window frames with highlights.</li> <li>• Stallboards</li> <li>• Verandah</li> </ul> <p>•Note: The face bricks have been overpainted and the verandah is missing.</p>
21-23	 <p>Source: Google Street View.</p>	<p><i>Contributory</i></p> <p><i>Significant Features</i></p> <p>Front elevation including</p> <ul style="list-style-type: none"> <li>• Parapet form ( may have been rendered over face brick)</li> <li>• Timber window frames</li> <li>• Ingo - recessed entry.</li> <li>• Stallboard</li> </ul> <p>• Note. ( This shop may have been identical to the one next door at 25 Railway place. ) The location and size of the above verandah signage is appropriate.</p>
25		<p><i>Contributory</i></p> <p><i>Significant Features</i></p> <p>Front elevation including</p> <ul style="list-style-type: none"> <li>• Face red brick walls</li> <li>• Parapet of red brick with contrasting rendered pilasters with flat caps, and a recessed sign panel.</li> <li>• Ingo - recessed entries.</li> <li>• Metal shop window frames with highlights in leadlight.</li> <li>• Stallboards with green glazed tiles</li> <li>• Piers with green glazed tiles and green glazed string course at the dado level.</li> <li>• Verandah</li> </ul> <p>•Note: Repointing the brickwork should be done with lime mortar, not cement mortar.</p>





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27	 <p>Source: Google Street View.</p>  	<p><i>Significant</i></p> <p><i>Significant Features</i></p> <ul style="list-style-type: none"> <li>• Built 1916 as a two storey unpainted brick bank and residence which is freestanding. The facade is highly articulated. It was built during WW1 and it is one of the earliest substantial buildings in the precinct.</li> <li>• The tall building is divided into narrow bays with projecting pilasters of brick and render which extend from ground level to the top of the high parapet.</li> <li>• Deep ornate cornices at ground and first floor ceiling levels, provide discrete horizontal lines.</li> <li>• Parapet articulated by rendered pilasters with flat tops and recessed rendered panels.</li> <li>• Timber windows at ground (hopper) and first floors (double hung with geometric lead light in the top) with deep horizontal and rendered lintels and sills.</li> <li>• The round arched doorway with a deep sinuous Art Nouveau hood mould over the rendered architrave and voussoirs is an exceptional architectural feature.</li> <li>• Double timber entry doors with round arched highlight above</li> <li>• Early electric lamp and metal bracket.</li> <li>• The tall red brick chimneys including the one visible from the side lane shown opposite. These are now a rare feature in the precinct.</li> <li>• Side lane access and the structures visible from the street boundary as shown in the photo adjacent.</li> <li>• Note: Some of the bricks and render have been overpainted. This could be removed chemically to restore the original unpainted finish and save on maintenance costs.</li> </ul>



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	<b>RAILWAY RESERVE</b>	
	  <p>Source. Google Street View</p>	<p><i>Significant</i>  <b>Railway Reserve</b></p> <p><i>Significant Features</i></p> <ul style="list-style-type: none"> <li>• Two mature palm trees</li> <li>• Station structures <ul style="list-style-type: none"> <li>• south side building and verandah</li> <li>• north side building and verandah</li> <li>• north and south platforms.</li> <li>• timber pedestrian bridge</li> <li>• signal box</li> </ul> </li> <li>• Various visual connections (views) to and from the Station structures and the historic places in Wingrove St, Station St, Gillies St and Railway Place.</li> <li>• The signal box is a key and prominent element of the substantially intact Federation era Fairfield station complex.</li> <li>• Situated at the southern entry to the Station Street shopping centre, the Railway Reserve complex, complete with signal box, imparts character to the centre and has become a local icon as an informal 'village square', valued by the community.</li> <li>• Note: The car parks, wire mesh fencing, gum trees and FIDO are all elements which are outside the main historic period of development that make up most of the heritage places in the precinct, eg 1880s to about 1940s and therefore they are not significant.</li> </ul>





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	<div data-bbox="268 304 719 562" data-label="Image"> </div> <p data-bbox="268 566 564 595">Source. Google Street View</p> <div data-bbox="268 658 719 994" data-label="Image"> </div> <p data-bbox="268 999 719 1122">The clear visual link of the signal box and the station buildings and train driver, was technically essential to operate the signals.</p>	<p data-bbox="738 309 858 338"><i>Significant</i></p> <ul data-bbox="738 349 963 380" style="list-style-type: none"> <li>• Signal Box c 1913</li> </ul> <p data-bbox="738 421 956 450"><i>Significant Features</i></p> <ul data-bbox="738 461 1426 1935" style="list-style-type: none"> <li>• Signal Box (1913 and closed in 2000.) with historical, aesthetic and technical significance. Corrugated hipped roof, two levels raised 3 metres above ground, weatherboards, elevated walkway supported on decorative metal brackets, timber lined eaves, fixed and sliding timber windows, a small section of original balustrade has survived by the entry door and is distinguished by its stop chamfered cross bracing.</li> <li>• The upper floor level there is a 26 lever frame with 10 levers occupying the space originally required for the 47 lever machine installed in 1913. There is space for a stove since removed, a locker, hand basin and standard fitments. Interior linings are beaded timber with double bull nosed architraves</li> <li>• Timber staircase.</li> <li>• Technically important (Criterion F) as an example of a machine having its origins in nineteenth century forms of railway safe working using mechanical interlocking and now superseded by digital technology.</li> <li>• It is historically important (Criterion A) as the earliest surviving signal box of its type in the metropolitan area and the equal oldest of its type in the State.</li> <li>• Aesthetically important (Criterion E) as a key and visually prominent element of the substantially intact Fairfield station complex of 1911, marking the point of entry to the Fairfield shopping centre from the south. Its rarity, however, (Criterion B), as a survivor of a once commonplace structure on the Victorian railway system enhances its value. The exposed cross braced frame recalls such nineteenth century industrial architectural forms as coal screens and mining head frames and was not commonly used with signal boxes.</li> <li>• Socially important (Criterion G) for the value placed on it by the local community and on the railway station complex generally, as is demonstrated by the recent adaptive works.</li> <li>• Earliest surviving signal box of its type in the metropolitan area.</li> <li>• It is integral with the station buildings and was always visually connected to it, to enable safe operations.</li> </ul>










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	 <p>Source. Google Street View</p>  	<p><i>Significant</i></p> <ul style="list-style-type: none"> <li>• Station Buildings c 1911 and platforms.</li> </ul> <p><i>Significant Features</i></p> <ul style="list-style-type: none"> <li>• Historically, aesthetically and socially significant.</li> <li>• Historically significant (Criterion A) as the passenger and goods station for its locality since construction in 1911, indirectly recalling its opening in 1888 and remaining in use for its original purpose at present.</li> <li>• Aesthetically significant (Criterion E) as an increasingly rare (Criterion B) substantially complete railway complex of the Post / Federation period (Criterion D), the Arts and Crafts motifs including strapwork, spandrels, terra cotta ridging, fibro cement slates, timber brackets, gable end vents, and rough cast work being hall marks of J. W. Hardy's office at the time.</li> <li>• Cantilevered verandahs, timber frame and weatherboards, timber casement windows.</li> <li>• Platforms on the north and south sides of the tracks.</li> </ul>
		<p><i>Significant</i></p> <ul style="list-style-type: none"> <li>• The footbridge c 1917</li> </ul> <p><i>Significant Features</i></p> <ul style="list-style-type: none"> <li>• The timber framed footbridge has steel main girders.</li> <li>• The steps and approach spans have timber framing with cross bracing characteristic for structures of the type.</li> <li>• Stairs, landings and balustrade posts.</li> <li>• Note: Alterations include reduction of the span of the bridge c1968, corrugated iron sheeting to the main span has replaced the pickets. The approach spans have wire mesh fences and tubular metal hand railing.</li> </ul>

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	 <p>Source. Google Street View</p>	<p><i>Significant</i></p> <ul style="list-style-type: none"> <li>• The two mature palms</li> </ul> <p><i>Significant Features</i></p> <ul style="list-style-type: none"> <li>• Aesthetic significance flanking the entrance to the station from Railway Place.</li> <li>• Visible from many parts of the precinct including on Wingrove Street.</li> <li>• Historically significant as they appear to date from the Federation era development of the station.</li> </ul>
	 <p>Source: <a href="#">Bourko</a> at <a href="#">English Wikipedia</a> 2008.</p>	<p><i>Not Significant.</i></p> <ul style="list-style-type: none"> <li>• The art work, FIDO is not significant as it was created in 2000 which is well outside the main historic period of development that make up most of the heritage places in the precinct, eg 1880s to about 1940s.</li> </ul>
	<b>STATION STREET</b>	
74	<p>NW Corner of Wingrove</p>  	<p><i>Not Significant.</i></p> <ul style="list-style-type: none"> <li>• The building(s) are not significant but the location is important in the heritage precinct as the gateway and pivot point to the historic precinct and visually linked to the railway reserve and its historic places.</li> <li>• The transparent corner facilitates the visual link between the historic railway and Station Street.</li> </ul>

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76		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Front elevation including</p> <ul style="list-style-type: none"> <li>• Tuck pointed brickwork.</li> <li>• Parapet rendered with flat capping,</li> <li>• Ingo - recessed entry.</li> <li>• Metal shop window frames with highlights in geometric Art Deco lead light.</li> <li>• Stallboards with appropriate black glazed tiles</li> <li>• Piers with black glazed tiles and glazed string course at the dado level.</li> <li>• Verandah</li> <li>• Tessellated tile ingo floor.</li> </ul> <p>•Note: Restoring the brickwork should be done by removing the paint with chemicals, and with lime mortar, not cement mortar.</p>
78		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Front elevation including</p> <ul style="list-style-type: none"> <li>• Parapet with curved central skyline, pilasters at each end, rendered with flat capping,</li> <li>• Verandah</li> </ul> <p>Note: The shop front below the verandah is altered. The location and size of the above verandah signage is appropriate.</p>








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80	 	<p><i>Contributory</i> <del>Significant</del></p> <p>One of a set of 4 identical shops: 80, 82, 84, 86</p> <p><i>Significant Features</i></p> <p>Front elevation including</p> <ul style="list-style-type: none"> <li>• Tuck pointed brickwork.</li> <li>• Parapet with brick pilasters, and rendered panels with flat capping,</li> <li>• Ingo - recessed entry.</li> <li>• Metal shop window frames with highlights in Art Nouveau lead light.</li> <li>• Stallboards</li> <li>• Verandah</li> <li>• Pressed metal ingo ceiling and terrazzo floor.</li> </ul> <p>•Note: Restoring the brickwork should be done by removing the paint with chemicals, and with lime mortar, not cement mortar.</p>
82	 	<p><i>Contributory</i></p> <p>One of a set of 4 identical shops: 80, 82, 84, 86</p> <p><i>Significant Features</i></p> <p>Front elevation including</p> <ul style="list-style-type: none"> <li>• Tuck pointed brickwork.</li> <li>• Parapet with brick pilasters, and rendered panels with flat capping,</li> <li>• Ingo - recessed entry.</li> <li>• Metal shop window frames with highlights in Art Nouveau lead light.</li> <li>• Stallboards.</li> <li>• Verandah</li> <li>• Pressed metal ingo ceiling and terrazzo floor.</li> </ul> <p>•Note: Restoring the brickwork should be done by removing the paint with chemicals, and with lime mortar, not cement mortar.</p>



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84		<p><i>Contributory</i> One of a set of 4 identical shops: 80, 82, 84, 86</p> <p><i>Significant Features</i> Front elevation including</p> <ul style="list-style-type: none"> <li>• Tuck pointed brickwork.</li> <li>• Parapet with brick pilasters, and rendered panels with flat capping,</li> <li>• Ingo - recessed entry.</li> <li>• Metal shop window frames with highlights.</li> <li>• Stallboards</li> <li>• Verandah</li> </ul> <p>•Note: Restoring the brickwork should be done by removing the paint with chemicals, and with lime mortar, not cement mortar. This shop front could be restored based on the intact shops at 80 and 82 Station St.</p>
85		<p><i>Contributory</i></p> <p><i>Significant Features</i> Front elevation including</p> <ul style="list-style-type: none"> <li>• Parapet</li> <li>• Verandah</li> </ul> <p>Note: Altered shop front below the verandah.</p>
86		<p><i>Contributory</i></p> <p><i>Significant Features</i> Front elevation including</p> <ul style="list-style-type: none"> <li>• Parapet with brick pilasters, and rendered panels with flat capping,</li> <li>• Verandah</li> </ul> <p>Note: Originally one of a set of 4 identical shops: 80, 82, 84, 86. This shop front could be restored based on the intact shops at 80 and 82 Station St. The shop front has been altered and a new first floor added towards the rear.</p>



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87 A, B, C, N		Not Significant
88A		Significant  HO190
88 B,C,D, E		Not Significant
88		Contributory  Significant Features Front elevation including <ul style="list-style-type: none"> <li>• Parapet with curved central skyline, pilasters at each end, rendered with flat capping,</li> <li>• Verandah</li> </ul> Note: The shop front below the verandah is altered.




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89-89A		Not Significant
90		Contributory.  Significant Features Front elevation including <ul style="list-style-type: none"> <li>• Parapet with curved central skyline, pilasters at each end, rendered with flat capping,</li> <li>• Verandah</li> </ul>
92-96		Not Significant




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91-97	  <p data-bbox="268 1375 718 1406">Source: Public Record Office, Victoria</p>	<p data-bbox="738 309 858 340"><i>Significant</i></p> <p data-bbox="738 378 957 409"><i>Significant Features</i></p> <p data-bbox="738 416 1038 448">Front elevation including</p> <ul data-bbox="738 454 1430 1444" style="list-style-type: none"> <li>• Built 1918 as the Bon Accord building, it is an exceptional freestanding two storey unpainted red brick structure, which is one of the earliest and substantial commercial buildings in the precinct. It is a landmark building with a highly articulated facade, grand window fenestration and a cantilevered verandah.</li> <li>• The long, tall building is divided into narrow vertical bays with projecting pilasters of unpainted red brick which extend to the stringcourse at the base of the tall parapet, whilst the end pilasters extend to the top of the parapet.</li> <li>• A refined horizontal balance to the facade is achieved with three projecting string courses, one at the impost level of the arched window openings, one at the base of the parapet and another at the top of the parapet.</li> <li>• Between the pilasters are very large timber windows with semi circular fan lights. The architrave follows the semicircular window frames in unpainted render with a keystone at the top. The render contrasts with the unpainted red brickwork to highlight the arched openings.</li> <li>• Unpainted red brick side walls.</li> <li>• Note: The bricks and render have been overpainted. This could be removed chemically to restore the original unpainted finish and save on maintenance costs. The ground floor below the verandah has been altered.</li> </ul>





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98-100		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Front elevation including</p> <ul style="list-style-type: none"> <li>• The very high parapet is a landmark feature which has simple horizontal and vertical articulation, and a cantilevered verandah.</li> <li>• The long, tall building is divided into three narrow vertical bays with projecting pilasters of render ( now painted). The top of the pilasters and along the top of the parapet panels is a simple flat rendered capping.</li> <li>• Between the pilasters are large expanses of plain render giving the it a modernist appearance.</li> <li>• Clerestory windows can be seen just above the verandah.</li> <li>• Note: This paint could be removed chemically to restore the original unpainted finish and save on maintenance costs. The ground floor below the verandah has been altered. The location and size of the above verandah signage is appropriate.</li> </ul>
102		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Front elevation including ( One of two identical ones)</p> <ul style="list-style-type: none"> <li>• Parapet of red brick with contrasting render on the flat capping and crenellated banding.</li> <li>• A large pediment finished with roughcast render surmounts the red brick parapet base..</li> <li>• Verandah</li> <li>• Note: Repointing the brickwork should be done with lime mortar, not cement mortar.</li> </ul> <p>Note: The shop front below the verandah is altered.</p>





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99-103	  	<p><i>Contributory</i></p> <p><i>Significant Features</i></p> <p>Front elevation including</p> <ul style="list-style-type: none"> <li>• Two storey group of three shops. The first floor merges with the parapet making the whole level above the verandah read as an extremely large parapet.</li> <li>• Parapet. Divided into three narrow bays by brick pilasters: the central bay has a flat skyline, but the two side bays have a very distinctive curved skyline edged with soldier bricks, between the face brick pilasters.</li> <li>• Face red brick- now painted.</li> <li>• Metal window frames at first floor level.</li> <li>• The northern most shop front which may have an original metal framed window and blue glazed tiles.</li> <li>• Verandah</li> </ul> <p>•Note: Restoring the brickwork should be done by removing the paint with chemicals, and with lime mortar, not cement mortar.</p> <p>There is evidence of a bricked up window as shown in the photograph opposite. This window is a thin high window with voussoirs forming a segmented arch above it. This indicates that the original facade was an earlier Federation era building which was altered in the late Inter-war era. Two shop fronts below the verandah are altered.</p>




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104		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Shop front including ( One of two identical ones)</p> <ul style="list-style-type: none"> <li>• Parapet of red brick with contrasting render on the flat capping and crenellated banding.</li> <li>• A large pediment finished with roughcast render surmounts the red brick parapet base.</li> <li>• Verandah</li> </ul> <p>•Note: Repointing the brickwork should be done with lime mortar, not cement mortar.</p> <p>Note: The shop front below the verandah is altered.</p>
105-107		<p><i>Not Significant</i></p> <p>(two shops)</p>
106		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Shop front including</p> <ul style="list-style-type: none"> <li>• Parapet with curved central skyline, pilasters at each end, rendered with flat capping,</li> <li>• Verandah</li> </ul> <p>Note: The shop front below the verandah is altered.</p>

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108		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Front elevation including</p> <ul style="list-style-type: none"> <li>• Two storey face red brick building including side walls visible from Station Street,</li> <li>• Parapet with curved central skyline, pilasters at each end, render and face brick banding, with flat capping,</li> <li>• First floor door and double hung timber windows, recessed balcony with timber slat balustrade.</li> <li>• Verandah</li> </ul> <p>Note: The shop front below the verandah is altered.</p>
110		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Front elevation including</p> <ul style="list-style-type: none"> <li>• Two storey face red brick building including side walls visible from Station Street,</li> <li>• Parapet with curved central skyline, pilasters at each end, render and face brick banding, with flat capping,</li> <li>• First floor timber window/door, recessed balcony with timber slat balustrade.</li> <li>• Verandah</li> <li>• Ground floor windows</li> <li>• Note: The original finish of unpainted render and brickwork, could be achieved with chemical removal of the paint.</li> </ul>
109-111A		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Elevations including</p> <ul style="list-style-type: none"> <li>• Two storey face red brick building including side walls visible from Station Street,</li> <li>• Parapet with curved central skyline, pilasters at each end, render and face brick banding, with flat capping,</li> <li>• First floor door and double hung timber windows, recessed balcony with timber slat balustrade.</li> <li>• Verandah</li> </ul> <p>Note: The shop front below the verandah is altered.</p>




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112-114		Not Significant
113		Not Significant
115-115A		Not Significant
116		Not Significant





Street No.	<b>Photograph</b> All photos are by Heritage Intelligence Pty Ltd, unless noted otherwise.	<b>Heritage Significance</b> As defined on page one of this Schedule.
117A-B		Not Significant
118		Contributory  Significant Features 1935 Elevations (one of a pair) including <ul style="list-style-type: none"> <li>• Parapet- rendered pilasters and recessed central panel, '1935' in relief, with flat top capping at the skyline.</li> <li>• Verandah</li> <li>• Note: New shop front below verandah. The parapet was originally unpainted render as seen in 120 adjacent. The paint could be removed chemically.</li> </ul>
119A, B		Not Significant
120		Contributory  Significant Features 1935 Elevations (one of a pair) including <ul style="list-style-type: none"> <li>• Parapet- unpainted rendered pilasters and recessed central panel, with 'BUILDINGS' in relief, with flat top capping at the skyline.</li> <li>• Verandah</li> <li>• Note: New shop front below verandah. The parapet has a good patina of age but if it needs to be cleaned it should be done with chemicals, not sand blasting.</li> </ul>










Street No.	<b>Photograph</b> All photos are by Heritage Intelligence Pty Ltd, unless noted otherwise.	<b>Heritage Significance</b> As defined on page one of this Schedule.
121-121A		<p><i>Contributory</i></p> <p><i>Significant Features</i> Two storey elevation including</p> <ul style="list-style-type: none"> <li>• Parapet - smooth rendered with a flat rendered top</li> <li>• First floor has a modernist design in which the parapet is 'read' as part of the first floor. The existing painted finish was originally three unpainted contrasting horizontal bands. From the verandah to the window sill level it is smooth render, between the sill and lintel level it is unpainted brick and above lintel height to the top of the parapet it is smooth render with flat capping in unpainted brick.</li> <li>• The first floor windows are unusual with delicate glazing bars, a fixed central pane and sliding windows either side. See figure opposite.</li> <li>• Verandah</li> <li>• Metal shop front windows with highlights.</li> <li>• Ingo - recessed entry</li> <li>• Note: The paint could be removed chemically from the bricks and render.</li> </ul>
84-90 Arthur Street		<p><i>Not Significant</i></p> <p>Fairfield Library complex</p>



Street No.	<b>Photograph</b> All photos are by Heritage Intelligence Pty Ltd, unless noted otherwise.	<b>Heritage Significance</b> As defined on page one of this Schedule.
122, 122A, 122B		<p><i>Contributory</i></p> <p><i>Significant Features</i></p> <p>Elevations (one of a pair) including</p> <ul style="list-style-type: none"> <li>• Parapet- pilasters with Art Deco detailing and central section, in unpainted tapestry bricks with a recessed and rendered central panel,</li> <li>• Verandah</li> <li>• Note: Altered front below verandah.</li> </ul>
123		<p><i>Contributory</i></p> <p><i>Significant Features</i></p> <p>Elevation including</p> <ul style="list-style-type: none"> <li>• First floor has a modernist design in which the parapet is 'read' as part of the first floor. The existing painted finish was originally unpainted brickwork. The top of the parapet flat capping in unpainted brick.</li> <li>• Verandah</li> <li>• Note: The first floor windows are altered. Altered front below verandah. Note: The paint could be removed chemically from the bricks and render.</li> </ul>
124		<p><i>Contributory</i></p> <p><i>Significant Features</i></p> <p>Elevation (one of a pair) including</p> <ul style="list-style-type: none"> <li>• Parapet- rendered pilasters with Art Deco detailing and central section with a recessed and rendered central panel and flat capping at the skyline.</li> <li>• Verandah</li> <li>• Note: Altered front below verandah. The paint could be removed chemically from the bricks and render.</li> </ul>

Street No.	<b>Photograph</b> All photos are by Heritage Intelligence Pty Ltd, unless noted otherwise.	<b>Heritage Significance</b> As defined on page one of this Schedule.
125		<p><i>Contributory</i></p> <p><i>Significant Features</i></p> <p>Elevation (one of a pair) including</p> <ul style="list-style-type: none"> <li>• Parapet- brick pilasters and central section with flat capping at the skyline.</li> <li>• Verandah</li> <li>• Metal shop front windows with highlights.</li> <li>• Ingo - recessed entry</li> <li>• Note: The paint could be removed chemically from the bricks and render.</li> </ul>
126	<p>SW Corner Duncan St.</p> 	<p><i>Contributory</i></p> <p><i>Significant Features</i></p> <p>Elevation (one of a pair) including</p> <ul style="list-style-type: none"> <li>• Parapet- rendered pilasters with Art Deco detailing and central section with a recessed and rendered central panel and flat capping at the skyline.</li> <li>• Verandah</li> <li>• Note: Altered front below verandah.</li> </ul>
127		<p><i>Contributory</i></p> <p><i>Significant Features</i></p> <p>Elevation (one of a pair) including</p> <ul style="list-style-type: none"> <li>• Parapet- brick pilasters and central section with flat capping at the skyline.</li> <li>• Verandah</li> <li>• Note: The paint could be removed chemically from the bricks and render. New shop front below verandah.</li> </ul>
	<p><b>WINGROVE STREET</b></p>	
254-256		<p><i>Not Significant</i></p>

Street No.	<b>Photograph</b> All photos are by Heritage Intelligence Pty Ltd, unless noted otherwise.	<b>Heritage Significance</b> As defined on page one of this Schedule.
262		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Elevations (one of a pair) including</p> <ul style="list-style-type: none"> <li>• Parapet- pilasters and central section rendered.</li> <li>• Note: The shop front below the parapet is altered.</li> </ul>
266		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Elevation (one of a pair) including</p> <ul style="list-style-type: none"> <li>• Parapet- pilasters and central section rendered.</li> <li>• Note: The shop front below the parapet is altered.</li> </ul>
268		<p><i>Contributory</i></p> <p><i>Significant Features</i>  Elevations facing Gillies and Wingrove Sts, including:</p> <ul style="list-style-type: none"> <li>• Parapet- rendered with four pilasters and balls on top, other decorative features including projecting rendered string courses, name plaque over splayed corner doorway flanked by long name plaques on both side elevations.</li> <li>• Tuck pointed brickwork ( now painted over)</li> <li>• Splayed corner with arched voussoirs entry doorway ( door is altered).</li> <li>• Ground floor window opening.</li> </ul> <p>•Note: The brickwork and render has been painted, which could be removed chemically - not by sand blasting. The window facing Wingrove St has been altered. There is a bricked up window on the Gillies St elevation.</p>

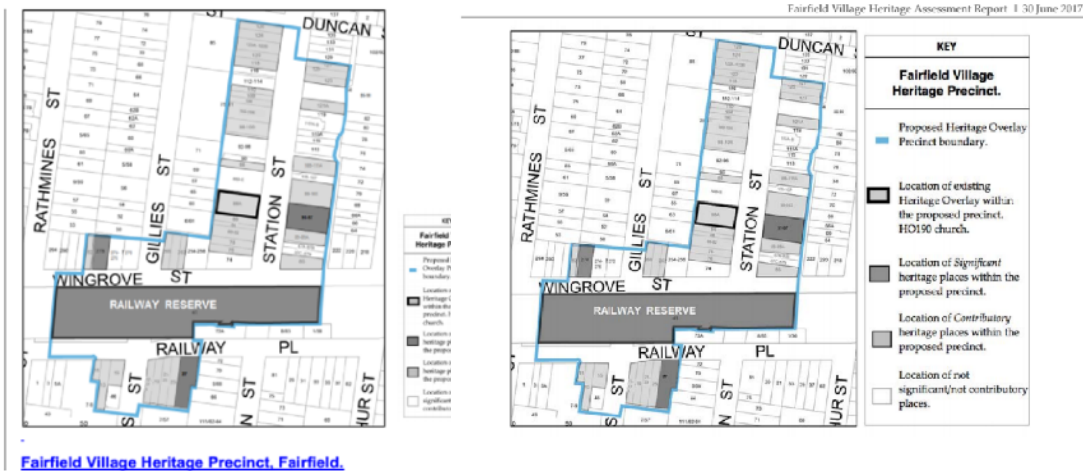
Street No.	<b>Photograph</b> All photos are by Heritage Intelligence Pty Ltd, unless noted otherwise.	<b>Heritage Significance</b> As defined on page one of this Schedule.
270		Not Significant
274-276		Not Significant
278		<p><i>Significant.</i></p> <p><i>Significant Features</i>            Elevations visible from Wingrove St, including:</p> <ul style="list-style-type: none"> <li>• Spanish Mission design. Roof, parapet, decoration, arches forming a loggia, fence, barley twist columns, timber windows.</li> <li>• The location and size of the above verandah signage is appropriate.</li> <li>• Refer to the detailed citation for more information.</li> </ul>
282		<p><i>Contributory</i></p> <p><i>Significant Features</i>            Elevation including</p> <ul style="list-style-type: none"> <li>• Parapet- with flat topped brickwork. .</li> <li>• Note: The shop front below the parapet is altered. Side walls may be terracotta lumber bricks.</li> <li>• The paint on the bricks could be removed chemically, not by sandblasting.</li> <li>• The location and size of the signage is appropriate.</li> </ul>



## 8.8. Additional information from the owners of 5-5A Railway Place.

(The following information was provided to Council after the public exhibition of the study.)

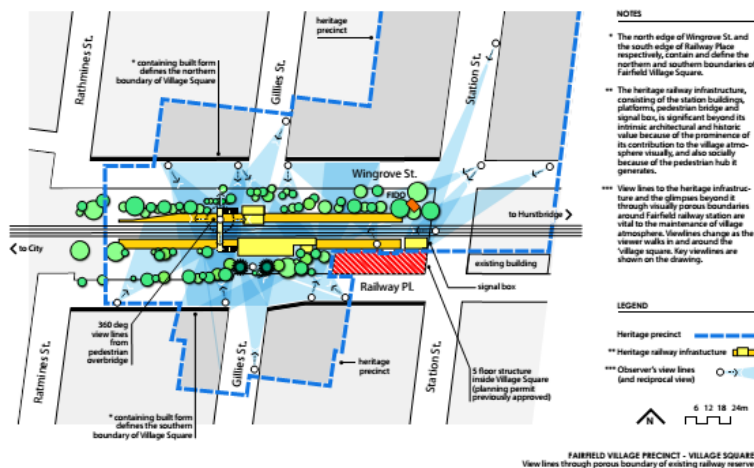
Introduction: 5 & 5a Railway Place Fairfield appears to be a late AND arbitrary inclusion in the Fairfield Village Heritage Precinct and the ASSOCIATED Heritage Assessment Report. The property does not appear in the Heritage Precinct Stage 1, nor the recommended amendments Stage 2 (see i below), nor the 'developed and documented' Fairfield Village Heritage Precinct Stage 2 (see ii below).



i. Recommended Amendments Stage 2

ii. Fairfield Village Heritage Precinct 'developed and documented' Stage 2.

Neither is the property listed in the Fairfield Village Heritage Precinct 'Village Square' 'developed and documented' Stage 2 (see Figure 3.12 below).



5 and 5a Railway Place is assessed in the Fairfield Village Heritage Assessment Report (June 2017) as a '...a very intact interwar shop...'. The assessment appears to have missed or overlooked the very significant changes to the premises' façade in the last 40 years.



Incorrect basis for citation: The citation of 5 and 5a Railway Place as a building of 'significant' heritage value to the precinct is an anomaly and its inclusion appears based on the fact that it is separated from the other 'contributory' heritage buildings in Railway Place by the multi-level Grocon development next door at 7-9 Railway Place. We understand that to include 5 and 5a Railway Place as a property with its own discrete heritage boundary, and adjacent to the Fairfield Village Heritage Precinct, the property's own heritage status has had to be elevated to 'significant'.

Rather, the 5 and 5a Railway Place building is of good, renovated condition, certainly not 'intact with historical integrity' as cited. For example:

- The three tiers of 'early glazed faience facing' have largely been replaced with similar coloured Chinese-made 'Steele Green' tiles from Schotts
- the stall-board tiles have been removed and the stall-boards rendered
- the original copper/brass window frames have been replaced with aluminium window frames
- the original ingoe doors insets have been replaced by flush doors on both sides
- The doors themselves have been replaced with wooden framed doors
- the original encaustic tiled floor entries have been removed and the area incorporated into the internal space
- the verandah has been rebuilt.

Renovated, adjacent to 'out of character in scale and height', 'visually isolated': Much of the changes above occurred in the 1970s, however, some were incorporated into a 2004 renovation, after the current owners bought and renovated a shabby and almost derelict shopfront. The owners acknowledge that their renovation is aesthetically pleasing and (intentionally) in harmony with the precinct; however, it is a modest, modernised, single storey shop/residence similar to many others throughout the City of Darebin (and indeed inner Melbourne). It is of an interesting aesthetic, rather than significant heritage category. There are no features of 5 and 5a Railway Place which set it apart (nor certainly not elevate its status) from the 'contributory' buildings in Railway Place, such as 11, 17-17a, 21-23, 25.



5 and 5a Railway Place late 2004



5 and 5a Railway Place early 2005

Any concept of 'significant' heritage value is further diminished by the adjacent development at 7-9 Railway Place. As the Fairfield Village Heritage Assessment Report itself states, 5-5a Railway Place is '...visually separated from other historic shops in Railway Place by a large development at 7-9 Railway Place...', '...which is significantly out of character in scale and height...', and '...visually isolates the last three properties in Railway Place.' (ie 5-5a, 3, and 1 Railway Place). This further diminishes the citation of 5 and 5a Railway Place as 'significant' and supports the exclusion of this property all together.

About Us. [ Deleted for privacy. ]

## ***8.9. City of Darebin Heritage Study Incorporated Plan - Permit Exemptions (2011) Recommended Amendments (2017)***

# CITY OF DAREBIN HERITAGE STUDY INCORPORATED PLAN – PERMIT EXEMPTIONS (2011) [Recommended Amendments 2017](#)

This incorporated plan sets out the permit exemptions from the provisions of the Heritage Overlay in accordance with Clause 43.01-2 that apply to specific heritage place and precincts assessed by the *City of Darebin Heritage Study – Historic Heritage Places* (2011), which were added to the Heritage Overlay by Amendment C108 (Part 1) to the Darebin Planning Scheme [and the \*Fairfield Village Heritage Assessment 2017\*, which were added to the Heritage Overlay by Amendment xxx to the Darebin Planning Scheme.](#)

Please refer to the relevant heritage place and precinct citations in the *City of Darebin Heritage Study – Historic Heritage Places* (2011) [and the \*Fairfield Village Heritage Assessment 2017\*](#), for further information about the significance of each place and precinct, including history, description and statements of significance. This information can also be accessed from the City of Darebin website or by contacting Darebin Council.

## 1 Application

These permit exemptions apply to places included within the Heritage Overlay as follows:

- Heritage precincts in the residential zones as specified in Table 3.1 (Section 3.1)
- Individual heritage places in the residential zones (Section 3.2)
- Heritage precincts in the business zones as specified in Table 3.2 (Section 3.3)
- East Plenty Tram Depot, 211-43 Plenty Road, Preston (Section 3.4)
- Whittlesea Railway Precinct (Section 3.5)
- Miller Road Tramway Bridge (Section 3.6)
- Northern Metropolitan Institute of Technology (Former Preston Technical School) (Section 3.7)
- [House and former doctor's surgery, 572 Plenty Road, Preston \(Section 3.8\)](#)
- [St Andrew's Alphington and Fairfield Uniting Church, 85 Gillies St, Fairfield \(Section 3.9\)](#)
- [Fairfield Railway reserve. \(Section 3.10\)](#)

This incorporated plan does not provide permit exemptions from a planning permit if required by any other provision of the Darebin Planning Scheme.

## 2 Definitions

The following definitions apply: *[Old definitions have been reformatted into a table with additions]*

### DEFINITIONS

<a href="#">Heritage Place</a>	<a href="#">Significant</a>	<a href="#">A Significant place is a single heritage place that has cultural heritage significance which may be independent of its context. These places may also contribute to the significance of a heritage precinct. Significant places within a heritage precinct will not usually have a separate Statement of Significance.</a>
	<a href="#">Contributory</a>	<a href="#">A Contributory place contributes to the significance of a heritage precinct, but would not be significant on their own.</a>

	<u>Significant feature</u>	<u>A Significant feature is any feature (building, tree, structure etc.) identified as contributing to the significance of a heritage place or precinct. Significant features are identified in the City of Darebin Heritage Study 2008 and the Fairfield Village Heritage Assessment 2017.</u>
<u>Non Heritage Place</u>	<u>Non-contributory or or Not Significant</u>	<u>Non-contributory or Not Significant places which do not contribute to the significance of a heritage precinct. In some instances, a Significant place may be considered Non-contributory or Not Significant within a precinct. For example, an important Modernist house within a Victorian era precinct.</u>
<u>Maintenance</u>		<u>Maintenance means the continuous protective care of a place, and its setting.</u> <u>Maintenance is to be distinguished from repair which involves restoration or reconstruction.</u>

Significant, Contributory and Non-contributory or Not Significant places within heritage precincts are shown on the precinct maps that form part of this incorporated plan – see Attachment A.

### 3 No Planning Permit Required

#### 3.1 Heritage precincts in the residential zones

This applies to the heritage precincts listed in Table 3.1, which are shown on the attached precinct maps. It does not apply to heritage places that are individually listed in the schedule to the Heritage Overlay (Refer to Section 3.2).

**Table 3.1 – Heritage precincts**

<b>Precinct</b>	<b>Locality</b>	<b>HO No.</b>
Broomfield Avenue	Alphington	HO297
Gladstone Avenue	Northcote	HO298
Carlisle Street	Preston	HO299
Garnet Street	Preston	HO300
Larne Grove and Roxburgh Street	Preston	HO302
Livingstone Parade	Preston	HO303
Milton Crescent	Preston	HO304
Edgar Street	Reservoir	HO306
Queen Street	Reservoir	HO308
Plow Street	Thornbury	HO310
Rossmoyne Street	Thornbury	HO311
Woolton Avenue	Thornbury	HO181

Under Clause 43.01-2 of the Planning Scheme, no planning permit is required for the following development within the heritage precincts subject to the Heritage Overlay specified in Table 3.1:

#### *Demolition and routine maintenance*

- Demolition of a building or part of a building on a property shown as *Non-contributory* on the relevant precinct map.
- Demolition of a side or rear fence. This does not apply to a property located on a corner or if any part of the side or rear fence is identified as a *Significant feature*.
- Repairs or routine maintenance to a building that would change the appearance of that building on a property shown as *Non-contributory* on the relevant precinct map. This does not apply if the repairs or routine maintenance would result in an extension to the building.
- Repairs or routine maintenance or alterations to the wall of a building that faces the rear boundary that would change the appearance of that building on a property shown as *Contributory* on the relevant precinct map. This does not apply if the repairs or routine maintenance would result in an extension to the building, or to a property on a corner site.

#### *Construction of and extensions to buildings, other structures, services and fences*

- Construction of an outbuilding with a gross floor area not more than 10 square metres and a maximum building height not more than 3 metres above natural ground level within the rear yard as defined in Figure 1.
- Construction or extension of an open-sided pergola or verandah with a finished floor level not more than 800mm above natural ground level and a maximum building height not more than 3 metres above natural ground level within the rear yard as defined in Figure 1.
- Construction or extension of a deck with a finished floor level not more than 800mm above natural ground level within the rear yard as defined in Figure 1.
- Construction of an extension to a building on a property shown as *Contributory* on the relevant precinct map provided that all of the following conditions are met:
  - the building height<sup>1</sup> is not more than the building height of the original dwelling excluding any later extensions or additions;
  - The extension is sited within the rear yard as defined in Figure 1.
  - the setback from side boundaries is not less than the setback of the existing building.
- Construction of an extension to a building on a property shown as *Non contributory* on the relevant precinct map provided that:
  - the building height<sup>1</sup> is not more than the building height of the existing building; and
  - the setback from front or side boundaries is not less than the setback of the existing building.
- Construction of a front fence not more than 1.2 metres in height above natural ground level provided that this does not require the demolition of an existing front fence of a property shown as *Significant* or *Contributory* on the relevant precinct map or identified as a *Significant feature* within the precinct.
- Construction of a side or rear fence including the installation of lattice or trellis. This exemption does not apply to:
  - Side fences within 3 metres of the frontage; or
  - Side fences along the secondary frontage of a property on a corner site.
- Installation of domestic services normal to dwelling on any property that may be visible from a street or public park provided that the installation:
  - is not attached to the front wall of the building;

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<sup>1</sup> "Building height" as defined by Clause 72 General Terms in the Darebin Planning Scheme



- is not situated between the front wall of the building and the front property boundary;
- if attached to the side wall of a building on a property shown as *Significant* or *Contributory* on the relevant precinct map, it is set back not less than 4 metres from the minimum front setback of the dwelling (See Note 1);
- does not project above the highest point of the roof;
- is not situated on that part of the roof that faces directly toward a street (including a side street); and if situated on part of a roof that faces a side boundary on a property shown as *Significant* or *Contributory* on the relevant precinct map, it is set back not less than 4 metres from the minimum front setback of the dwelling (See Note 1).
- Construction or extension of a domestic swimming pool or spa and associated mechanical equipment and safety fencing on any property provided that the pool is situated within the rear yard as defined in Figure 1

### **3.2 Individual places in the residential zones**

This applies to heritage places that are individually listed in the Schedule to the Heritage Overlay, except for 572 Plenty Road, Preston (Refer to Section 3.8).

Under Clause 43.01-2 of the Planning Scheme, no planning permit is required for the following development for individually listed heritage places within the residential zones subject to the Heritage Overlay:

#### *Demolition and routine maintenance*

- Demolition of or alterations to a building that is not specified as a *Significant feature*. This includes routine maintenance that would change the appearance of a building.
- Demolition of a side or rear fence. This does not apply to a property located on a corner or if any part of the side or rear fence is identified as a *Significant feature*.

#### *Construction of and extensions to buildings and fences*

- Construction of an outbuilding with a gross floor area not more than 10 square metres and a maximum building height not more than 3 metres above natural ground level within the rear yard of a property as defined in Figure 1. This does not apply if it would require the removal, demolition or alteration of a *Significant feature*.
  - Construction or extension of an open-sided pergola or verandah with a finished floor level not more than 800mm above natural ground level and a maximum building height not more than 3 metres above natural ground level within the rear yard as defined in Figure 1. This does not apply if it would require the removal, demolition or alteration of a *Significant feature*.
  - Construction or extension of a deck with a finished floor level not more than 800mm above natural ground level within the rear yard as defined in Figure 1. This does not apply if it would require the removal, demolition or alteration of a *Significant feature*.
  - Construction of a front fence not more than 1.2 metres in height above natural ground level provided that this does not require the demolition of an existing front fence that is identified as a *Significant feature*.
  - Construction of a side or rear fence including the installation of lattice or trellis. This exemption does not apply to:
    - Side fences within 3 metres of the frontage; or
    - Side fences along the secondary frontage of a property on a corner site.
    - The installation of lattice or trellis on a fence identified as a *Significant*
-

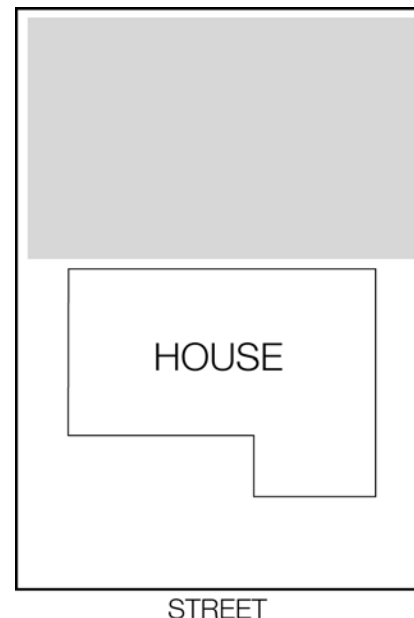
feature.

- Construction or extension of a domestic swimming pool or spa and associated mechanical equipment and safety fencing on any property provided that the pool is situated within the rear yard as defined on Figure 1. This does not apply if it would require the removal, demolition or alteration of a *Significant feature*.

**NOTE 1:** *For the purposes of this exemption the front setback is measured to the original dwelling and not to any later extensions or additions such as garages or carports*

**FIGURE 1**

*The shaded area defines the rear yard for the purposes of this policy*



### 3.3 Heritage precincts in the business zones

This applies to the heritage precincts listed in Table 3.2, which are shown on the attached precinct maps.

**Table 3.2 – Heritage precincts**

Precinct	Locality	HO No.
High Street, Preston	Preston	HO301
Plenty Road	Preston	HO305
High Street, Reservoir	Reservoir	HO307
High Street, Thornbury	Thornbury	HO309
<a href="#">Fairfield Village</a>	<a href="#">Fairfield</a>	<a href="#">HOxxx</a>

Under Clause 43.01-2 of the Planning Scheme, no planning permit is required for the following development within the heritage precincts subject to the Heritage Overlay listed in Table 3.2:

- Demolition of a building or part of a building on a property shown as *Non-contributory* [or Not Significant](#) on the relevant precinct map.
- Routine maintenance to a building that would change the appearance of that building on a property shown as *Non-contributory* [or Not Significant](#) on the relevant precinct map.
- Signage situated below verandah at ground floor level on a building on a property shown as *Non-contributory* [or Not Significant](#) on the relevant precinct

plan.

- Above verandah signage on a building on a property shown as *Non-contributory* on the relevant precinct plan unless the building is adjacent to a *Significant* or *Contributory* building as shown on the relevant precinct map.
- Installation of an automatic teller machine on a building on a property shown as *Non-contributory* or Not Significant on the relevant precinct plan.
- The alteration to an existing building façade of a building on a property shown as *Non-contributory* or Not Significant on the relevant precinct plan provided that:
  - The alteration does not include the installation of an external roller shutter
  - At least 80 per cent of the building front at ground level is maintained as an entry or window with clear glazing.
- An awning on a building on a property shown as *Non-contributory* or Not Significant on the relevant precinct plan that projects over a public road reservation if it is authorised by the relevant public land manager.

### 3.4 East Preston Tram Depot

This applies to the East Preston Tram Depot at 211-243 Plenty Road, Preston (HO240). *Significant features* are listed in Table 3.3.

**Table 3.3 – East Preston Tram Depot significant features**

Significant feature	Non-significant features
The offices, constructed by 1955, facing Plenty Road	Later alterations and additions
The covered tram storage shed, constructed by 1955, to the extent of the exterior walls and roof	Later alterations and additions

Under Clause 43.01-2 of the Planning Scheme, no planning permit is required for the following demolition and buildings and works within HO240 (East Preston Tram Depot):

- Removal of, modifications and repairs to and replacement of overhead power lines.
- Repairs to and replacement of tramway tracks.
- Modifications and repairs to and replacement of any electric or electronic signalling equipment.
- Removal of, repairs to, installation or replacement of safety barriers, rubbish bins, seating, bicycle racks and other small items of furniture.
- Removal, installation, repair or replacement of non-commercial signage, security lighting, fire safety equipment provided it does not involve the removal or erection of a building or other structure.
- Resurfacing of existing paths and driveways.
- Removal, installation or replacement of garden watering, water recycling or drainage systems.
- Management of trees in accordance with Australian Standard 4373: Pruning of Amenity Trees.

### 3.5 Whittlesea Railway Precinct

This applies to all sites included within the Whittlesea Railway Precinct (HO295).

Under Clause 43.01-2 of the Planning Scheme, no planning permit is required for the following demolition and buildings and works within HO295 (Whittlesea Railway Precinct):

- Removal of, modifications and repairs to and replacement of overhead power lines.
- Repairs to and replacement of railway tracks and sleepers including ballast.
- Modifications and repairs to and replacement of any electric or electronic signalling equipment.
- Removal of, repairs to, installation or replacement of ticket machines, passenger control gates, safety barriers, rubbish bins, seating, bicycle racks and other small items of platform furniture.
- Removal, installation, repair or replacement of non-commercial signage, security lighting, fire safety equipment provided it does not involve the removal or erection of a building or other structure.
- Resurfacing of existing paths and driveways.
- Removal, installation or replacement of garden watering, water recycling or drainage systems.
- Management of trees in accordance with Australian Standard 4373: Pruning of Amenity Trees.
- The construction or demolition of buildings and works and tree removal necessary to achieve compliance with the *Disability Discrimination Act 1992* and *Disability Standards for Accessible Public Transport 2002* in accordance with a plan for such works within the Heritage Overlay area which has been approved by the responsible authority.

### 3.6 Miller Street Tramway Bridge

This applies to the Miller Street Tramway Bridge (HO 236). *Significant features* are listed in Table 3.4.

**Table 3.4 – Miller Street Tramway Bridge significant features**

Significant feature	Non-significant features
Brick abutments either side of the railway line	Post 1945 alterations and additions including concrete deck, supporting piers and cyclone wire fencing.

Under Clause 43.01-2 of the Planning Scheme, no planning permit is required for the following demolition and buildings and works within HO236 (Miller Street Tramway Bridge):

- Alterations or additions, or routine maintenance to a structure that would change the appearance of that structure other than the *Significant features* of the structure listed in Table 3.4.
- Installation of or alterations and additions to trackwork, overhead wiring and associated infrastructure and the carrying out of associated works.

### 3.7 Former Preston Technical College (NMIT)

This applies to the former Preston Technical College (now NMIT) 77-89 St Georges Road, Preston (HO250). *Significant features* are listed in Table 3.5.

**Table 3.5 – Former Preston Technical College (NMIT) significant features**

Significant feature	Non-significant features
The former Preston Technical College designed by Percy Everett and constructed by 1937	Later alterations and additions including the additions to the north and south wings constructed c.1955.  Landscaping and other buildings.

Under Clause 43.01-2 of the Planning Scheme, no planning permit is required for the following demolition and buildings and works within HO250 (Former Preston Technical College):

- Demolition or alteration of non-significant buildings or features. Construction or demolition of buildings and works necessary to achieve compliance with the *Disability Discrimination Act 1992* in accordance with a plan for such works within the Heritage Overlay area that has been approved by the responsible authority.
- Construction of a fence not more than 1.2 metres in height above natural ground level.
- Construction or extension of a deck with a finished floor level not more than 800mm above natural ground level.
- Removal of, repairs to, installation or replacement of safety barriers, rubbish bins, seating, bicycle racks and other small items of furniture.
- Removal, installation, repair or replacement of non-commercial signage, security lighting, fire safety equipment provided it does not involve the removal or erection of a building or other structure.
- Resurfacing of existing paths and driveways or construction of new paths or driveways.
- Removal, installation or replacement of garden watering, water recycling or drainage systems.
- Management of trees in accordance with Australian Standard 4373: Pruning of Amenity Trees.

### 3.8 House, garage and former doctor's surgery, 572 Plenty Road, Preston

This applies the house, garage and former doctor's surgery, 572 Plenty Road, Preston (HO242). *Significant features* are listed in Table 3.6.

**Table 3.6 – House, garage and former doctor's surgery significant features**

Significant feature	Non-significant features
The house and its interior designed by Harold Desbrowe Annear.	The interior of the garage.
The garage designed by Harold Desbrowe Annear.	The side and rear fencing other than the arched gateway and high rendered wall.
The arched gateway and high rendered wall adjoining the house facing Plenty Road	

Under Clause 43.01-2 of the Planning Scheme, no planning permit is required for



the following buildings and works within HO242:

#### *Interior*

- Painting of previously painted walls and ceilings provided that preparation or painting does not remove evidence of any original paint or other decorative scheme.
- Installation, removal or replacement of carpets and/or flexible floor coverings.
- Installation, removal or replacement of curtain tracks, rods and blinds.
- Installation, removal or replacement of hooks, nails and other devices for the hanging of mirrors, paintings and other wall mounted art.
- Refurbishment of existing bathrooms, toilets and kitchens including removal, installation or replacement of sanitary fixtures including the original shower structure and associated piping, mirrors, wall and floor coverings.
- Demolition or removal of non-original bathroom partitions and tiling, sanitary fixtures and fittings, kitchen wall tiling and equipment, lights, built-in cupboards and the like.
- Removal or replacement of non-original door and window furniture including, hinges, locks, knobsets and sash lifts.
- Installation of stud walls, which are removable, providing no alteration to the structure is required.
- Removal of tiling or concrete slabs in wet areas provided there is no damage to or alteration of original structure or fabric.
- Installation, removal or replacement of ducted, hydronic or concealed radiant type heating provided that the installation does not damage existing skirtings and architraves and that the central plant is concealed.
- Installation, removal or replacement of electrical wiring provided that all new wiring is fully concealed and any original servant's bells, light switches, pull cords, push buttons or power outlets are retained in-situ. Note: if wiring original to the place was carried in timber conduits then the conduits should remain in situ.
- Installation, removal or replacement of bulk insulation in the roof space.
- Installation of plant within the roof space.
- Installation of new built-in cupboards providing no alteration to the structure is required.

#### *Demolition and routine maintenance*

- Demolition of or alterations to a building that is not specified as a *Significant feature*. This includes routine maintenance that would change the appearance of a building.
- Demolition of the non-significant side or rear fence.

#### *Construction of and extensions to buildings and fences*

- Construction of an outbuilding with a gross floor area not more than 10 square metres and a maximum building height not more than 3 metres above natural ground level within the rear yard of a property as defined in Figure 1. This does not apply if it would require the removal, demolition or alteration of a *Significant feature*.
  - Construction or extension of an open-sided pergola or verandah with a finished floor level not more than 800mm above natural ground level and a maximum building height not more than 3 metres above natural ground level within the rear yard as defined in Figure 1. This does not apply if it would require the removal, demolition or alteration of a *Significant feature*.
-

- Construction or extension of a deck with a finished floor level not more than 800mm above natural ground level within the rear yard as defined in Figure 1. This does not apply if it would require the removal, demolition or alteration of a *Significant feature*.
- Construction of a side or rear fence including the installation of lattice or trellis. This exemption does not apply to the installation of lattice or trellis on a fence identified as a *Significant feature*.
- Construction or extension of a domestic swimming pool or spa and associated mechanical equipment and safety fencing on any property provided that the pool is situated within the rear yard as defined on Figure 1. This does not apply if it would require the removal, demolition or alteration of a *Significant feature*.

### 3.9 St Andrew's Alphington and Fairfield Uniting Church, 85 Gillies St, Fairfield

This applies St Andrew's Alphington and Fairfield Uniting Church, 85 Gillies St, Fairfield, (HOxx). *Significant features* are listed in Table 3.8

**Table 3.8 – St Andrew's Alphington and Fairfield Uniting Church significant features**

<u>Significant feature</u>	<u>Non-significant features</u>
<u>The exterior and interior of the church as designed by Francis Bruce Kemp.</u>	<u>The existing picket fencing on the street boundaries</u>  <u>The features located within the blue polygon on the aerial plan in Attachment B</u>

Under Clause 43.01-2 of the Planning Scheme, no planning permit is required for the following buildings and works within HOxx:

#### Interior

- Painting of previously painted walls and ceilings provided that preparation or painting does not remove evidence of any original paint or other decorative scheme.
- Installation, removal or replacement of carpets and/or flexible floor coverings. Installation, removal or replacement of curtain tracks, rods and blinds.
- Installation, removal or replacement of hooks, nails and other devices for the hanging of mirrors, paintings and other wall mounted art.
- Refurbishment of existing bathrooms, toilets and kitchens including removal, installation or replacement of sanitary fixtures including and associated piping, mirrors, wall and floor coverings.
- Demolition or removal of non-original partitions and tiling, sanitary fixtures and fittings, wall tiling and equipment, lights, built-in cupboards and the like.
- Removal or replacement of non-original door and window furniture including, hinges, locks, knobsets and sash lifts.
- Installation of stud walls, which are removable, providing no alteration to the structure is required.
- Removal of tiling or concrete slabs in wet areas provided there is no damage to or alteration of original structure or fabric.
- Installation, removal or replacement of ducted, hydronic or concealed radiant type heating provided that the installation does not damage existing skirtings and architraves and that the central plant is concealed.

- Installation, removal or replacement of electrical wiring provided that all new wiring is fully concealed.
- Installation, removal or replacement of bulk insulation in the roof space.
- Installation of plant within the roof space.
- Installation of new built-in cupboards providing no alteration to the structure is required.

#### Demolition and routine maintenance

- Demolition of or alterations to all features within the blue polygon shown in Figure 2. This includes routine maintenance that would change the appearance of a building.
- Demolition of the non-significant side or rear fences.
- Construction of and extensions to buildings and fences

FIGURE 2

St Andrew's Alphington and Fairfield  
Uniting Church, 85 Gillies St, Fairfield  
The red line is the HO boundary and  
the blue polygon is the area that is  
appropriate for future development.



### 3.10 Fairfield Railway Reserve

This applies to all land within the curtilage of the Fairfield Railway reserve as shown on the Fairfield Village Heritage Precinct map in Attachment A. Significant features are listed in Table 3.9.

**Table 3.9 – Fairfield Railway Reserve significant features**

<u>Significant feature</u>	<u>Non-significant features</u>
<u>The two mature palm trees at entrance to station from Railway Place</u>	<u>Wire mesh fencing</u>
<u>South side station building and verandah</u>	<u>Car park areas</u>
<u>North side station building and verandah</u>	<u>Gum trees</u>
<u>North and south platforms</u>	<u>FIDO art work</u>
<u>Timber pedestrian bridge</u>	<u>South side Protective Services Officer building and adjacent shelter</u>
<u>Signal Box, including timber staircase</u>	

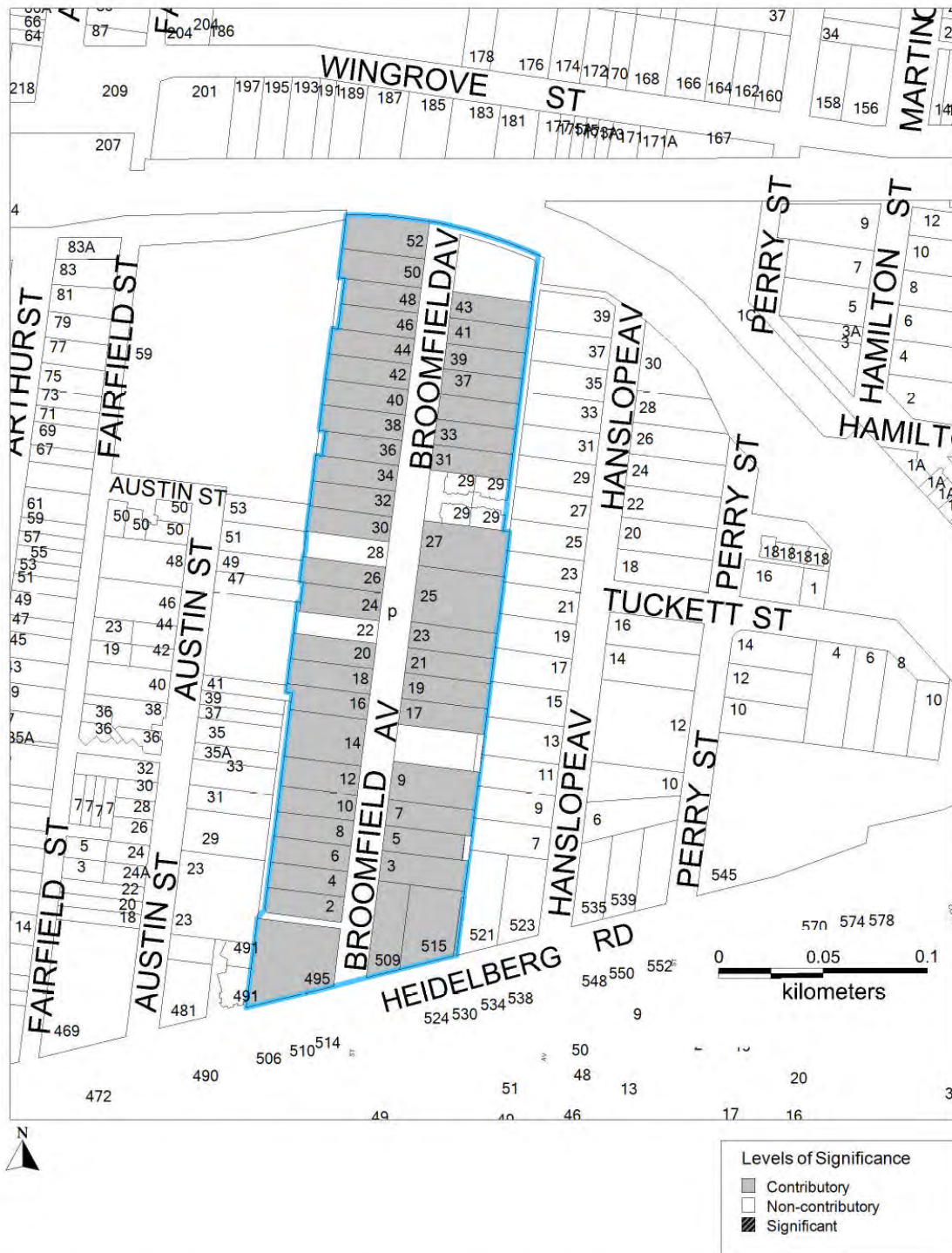
Under Clause 43.01-2 of the Planning Scheme, no planning permit is required for the following demolition and buildings and works within the Fairfield Railway Reserve:

- Removal of, modifications and repairs to and replacement of overhead power lines and associated support structures.
- Works, repairs and routine maintenance which change the appearance of a building, structure, tree or other item not identified as a significant feature in Table 3.9.
- Works, repairs and routine maintenance which do not change the appearance of a building, structure, tree or other item identified as a significant feature in Table 3.9.
- Repairs to and replacement of railway tracks and sleepers including ballast.
- Modifications and repairs to and replacement of any signaling or communications equipment.
- Removal of, repairs to, installation or replacement of ticket machines, passenger control gates, safety barriers, rubbish bins, seating, bicycle racks and other small items of platform furniture.
- Removal, installation, repair or replacement of non-commercial signage, security lighting and fire safety equipment provided it does not involve the removal or erection of a building or other structure.
- Demolition or removal of a fence
- Resurfacing of existing paths, platforms and driveways provided this is undertaken to the same details, specifications and materials.
- Removal, installation or replacement of garden watering, water recycling or drainage systems.
- Management of trees (except the two palm trees) in accordance with Australian Standard 4373: Pruning of Amenity Trees.
- The construction or demolition of buildings and works and tree removal necessary to achieve compliance with the Disability Discrimination Act 1992 and Disability Standards for Accessible Public Transport 2002, except for demolition or removal of any item identified as a significant feature in Table 3.9.

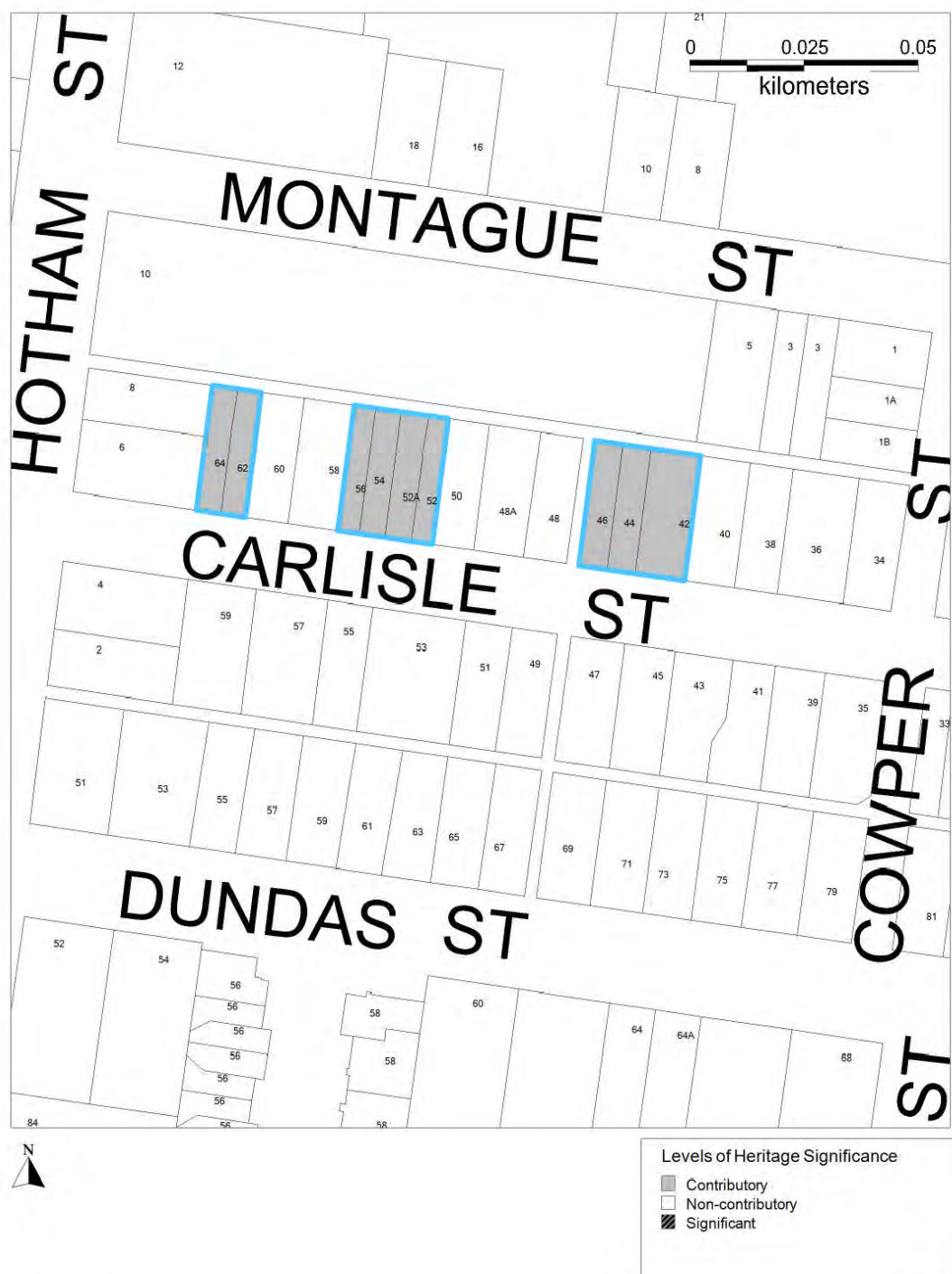
# **ATTACHMENT A - HERITAGE PRECINCT MAPS**







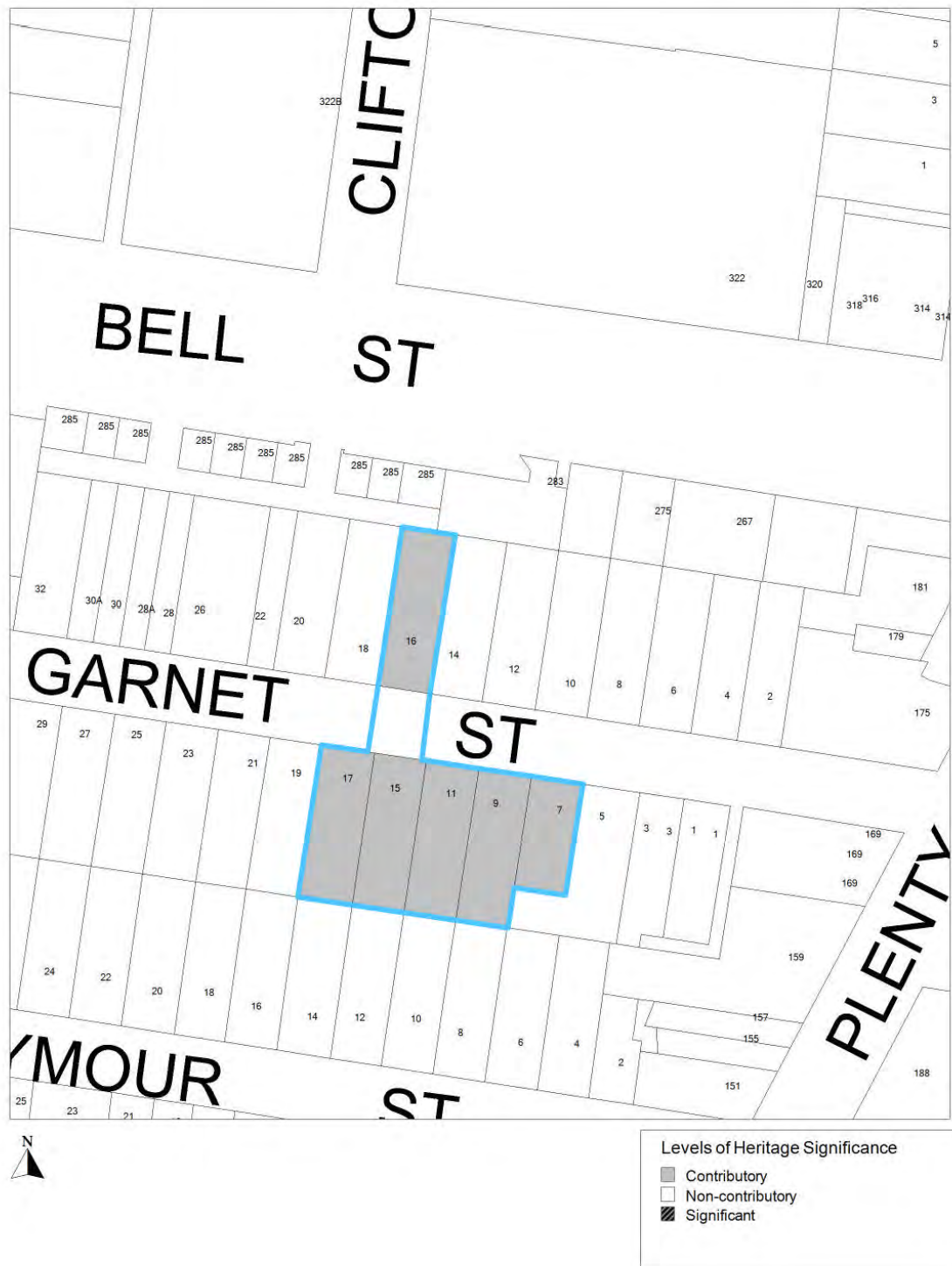
## Broomfield Avenue Precinct, Alphington



**Carlisle Street Precinct, Preston**

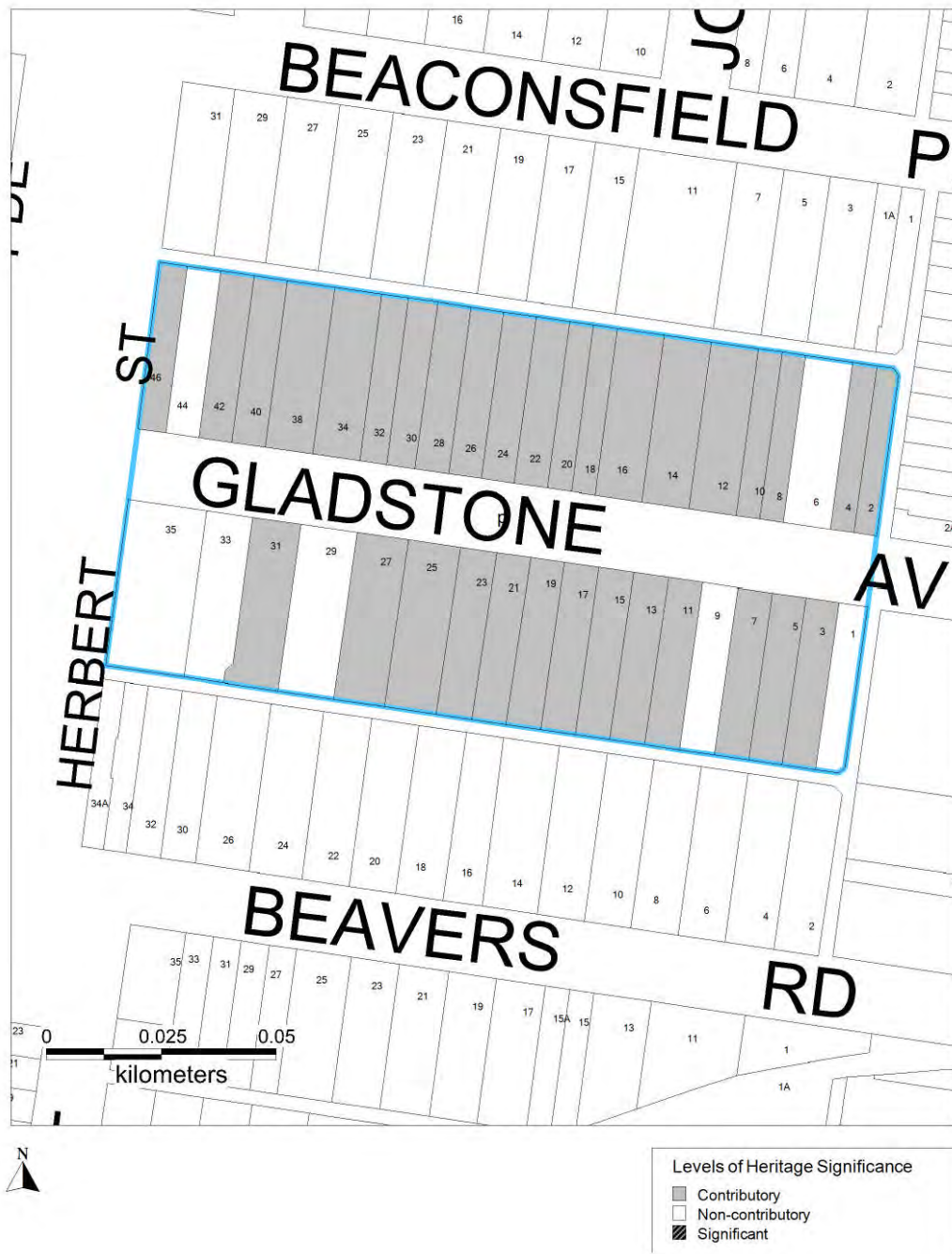


## Edgar Street Precinct, Reservoir



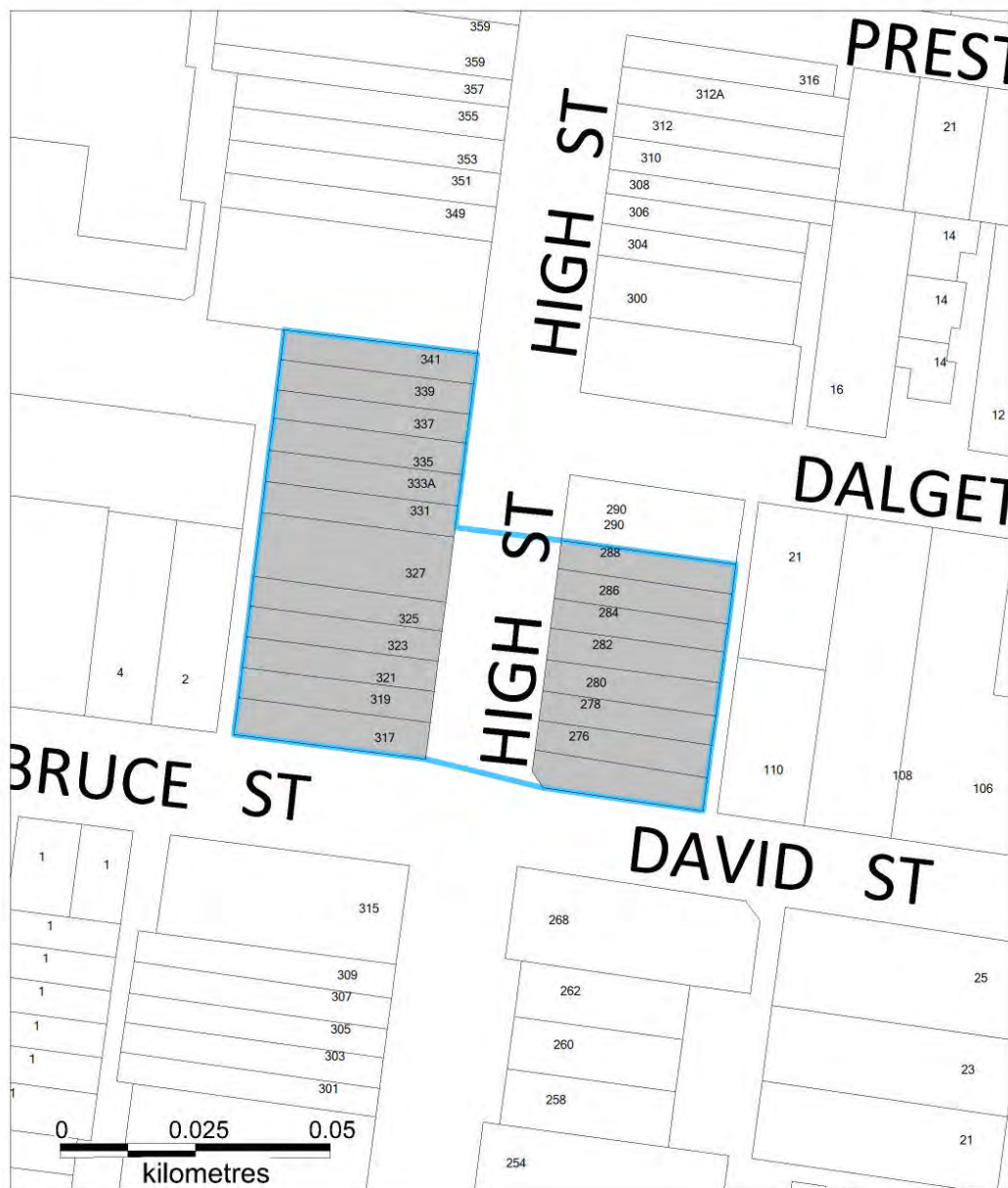
## Garnet Street Precinct, Preston





## Gladstone Avenue Precinct, Northcote



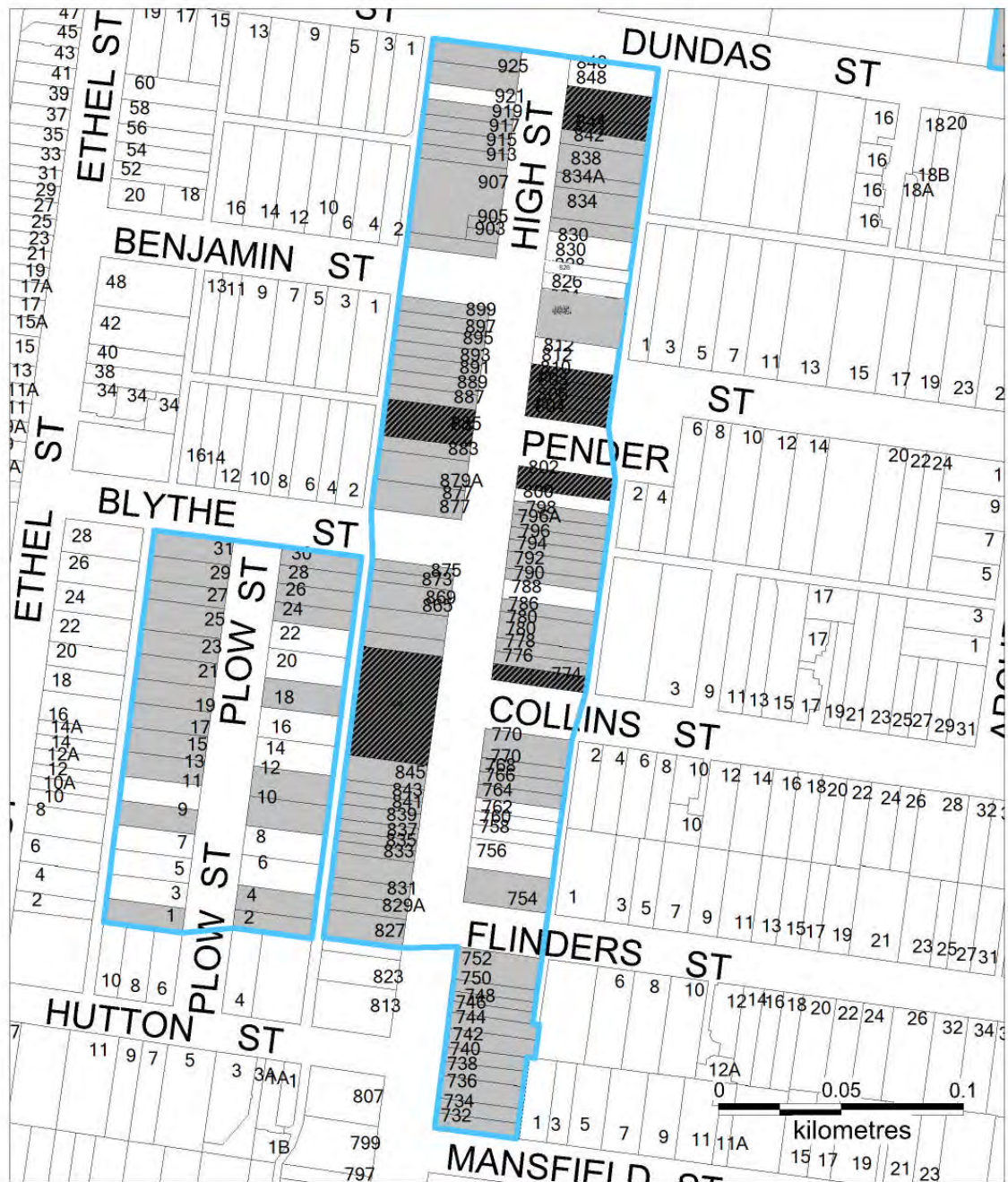


**Levels of Heritage Significance**

- Contributory
- Non-contributory
- Significant

## High Street, Preston



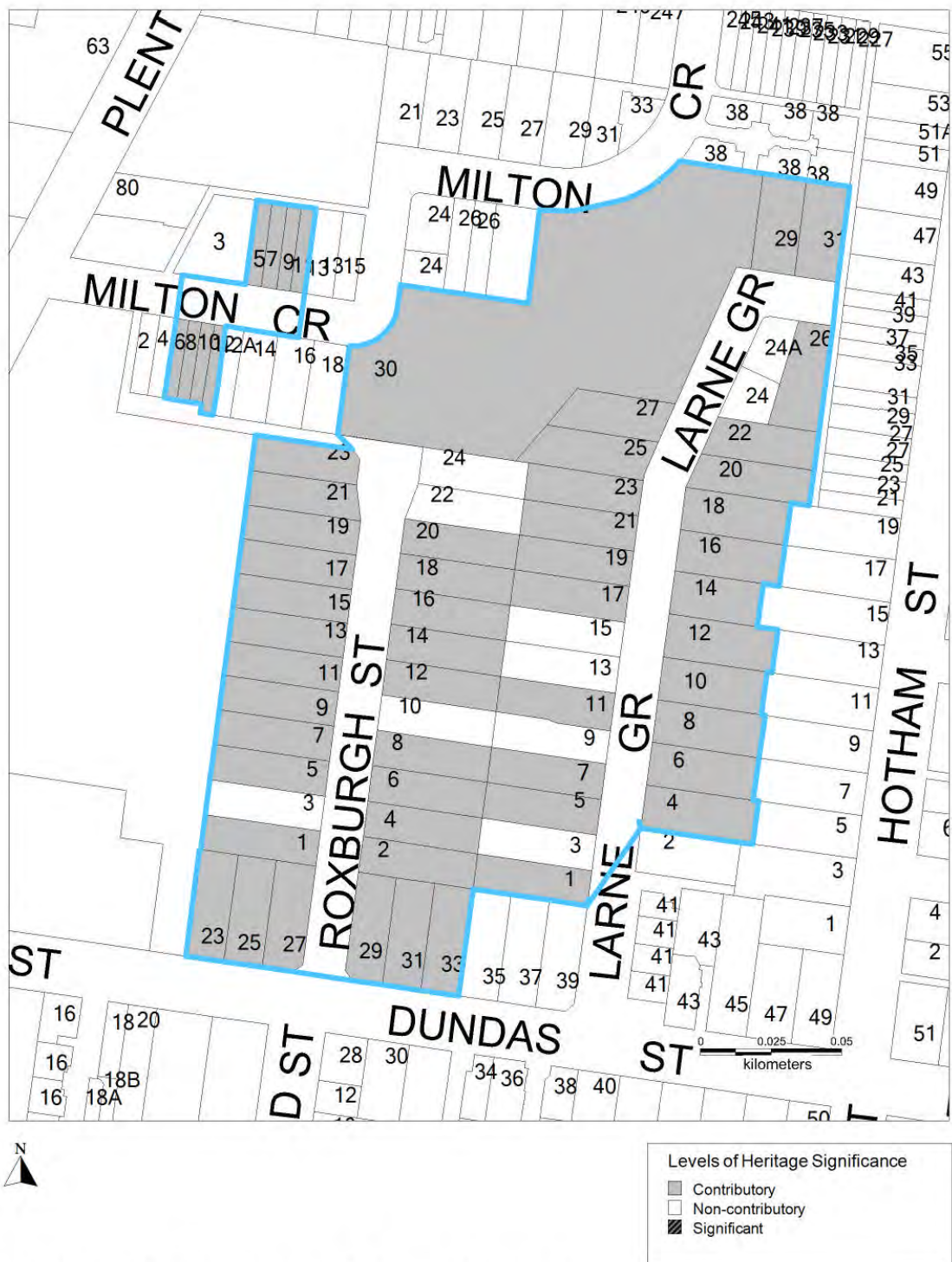


## High Street, Thornbury





## Livingstone Parade Precinct, Preston

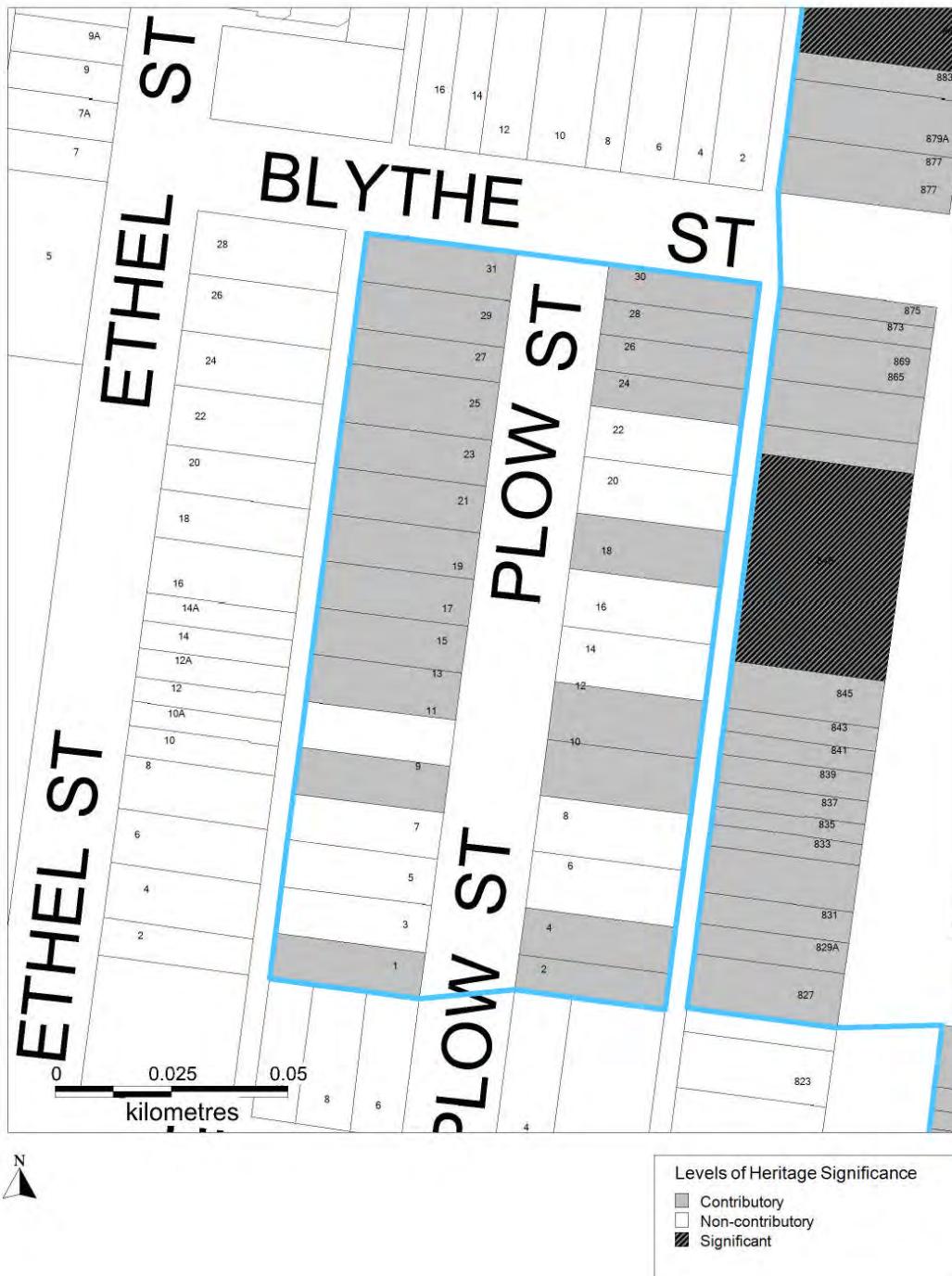


## Milton Crescent Precinct & Larne Grove and Roxburgh Street Precinct, Preston

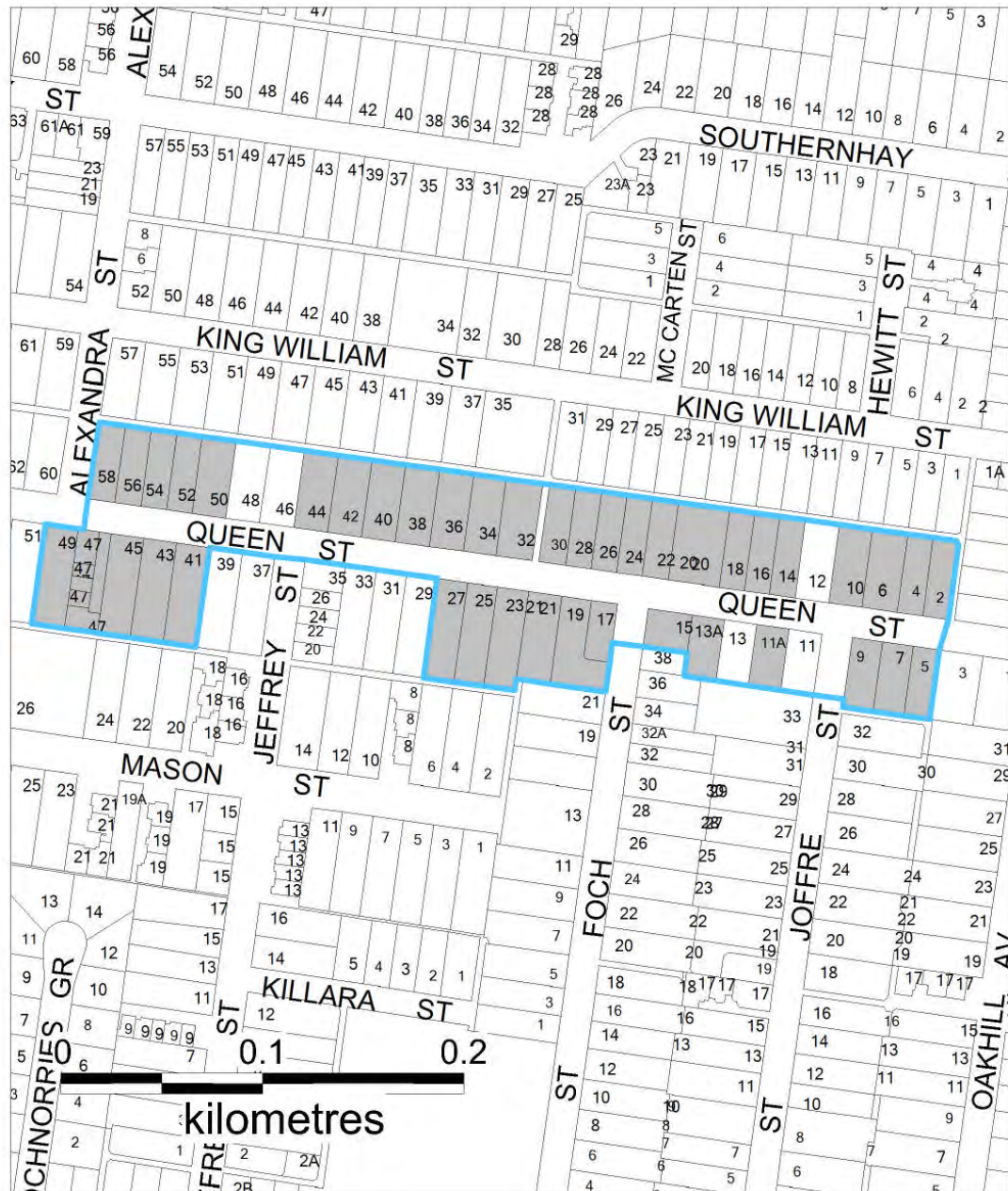




## Plenty Road, Preston



## Plow Street Precinct, Thornbury

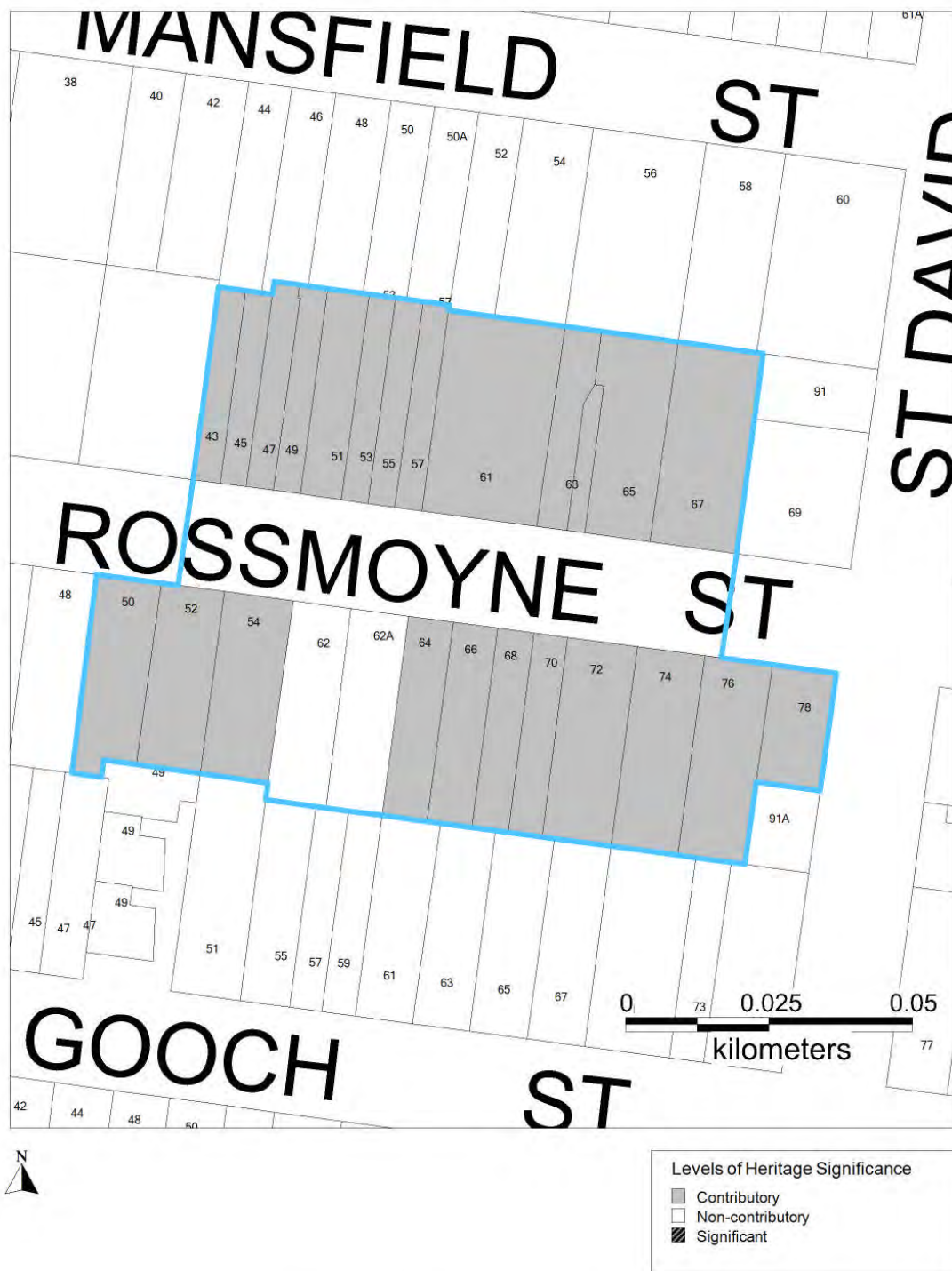


Levels of Heritage Significance

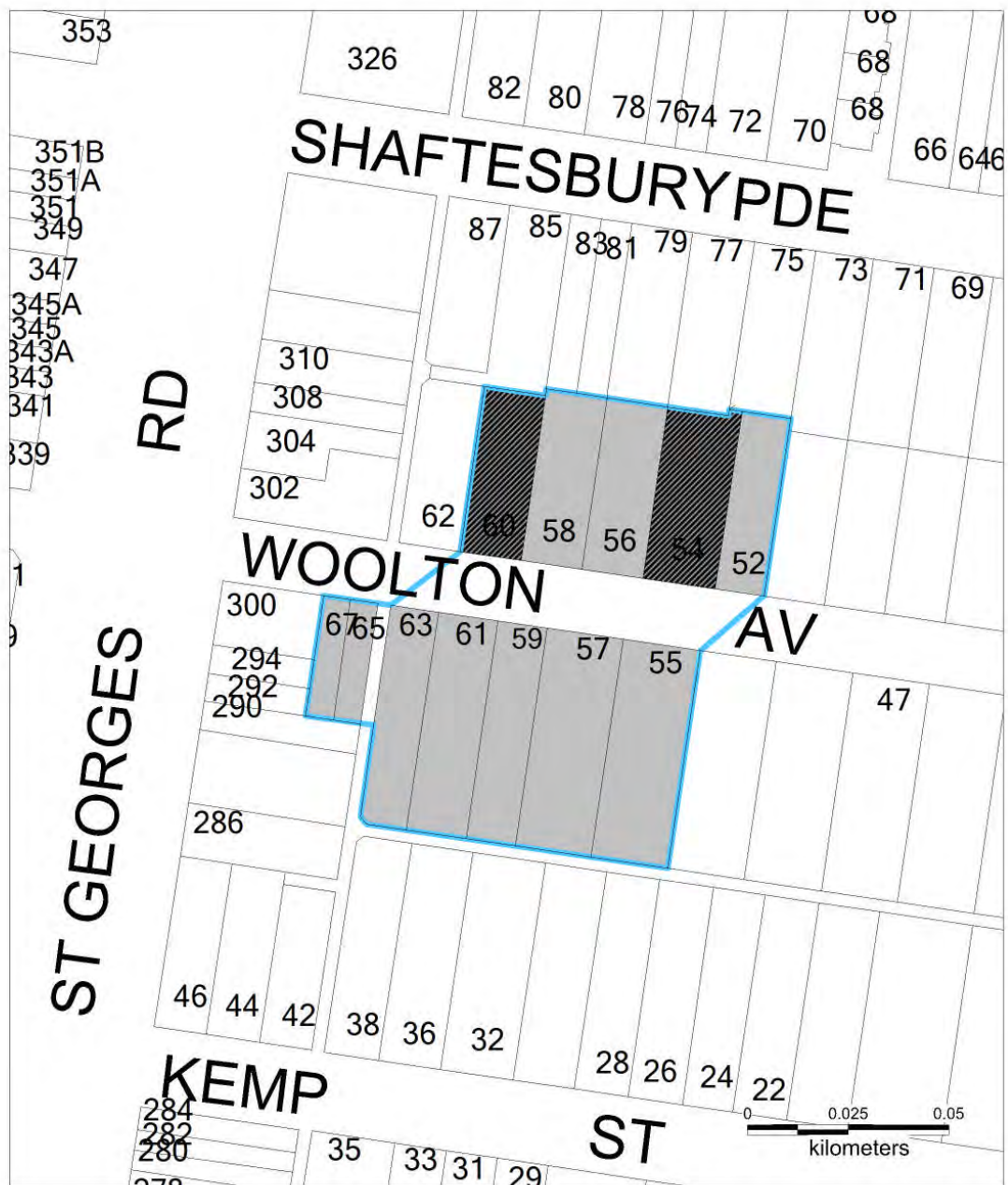
- Contributory
- Non-contributory
- Significant

## Queen Street Precinct, Reservoir



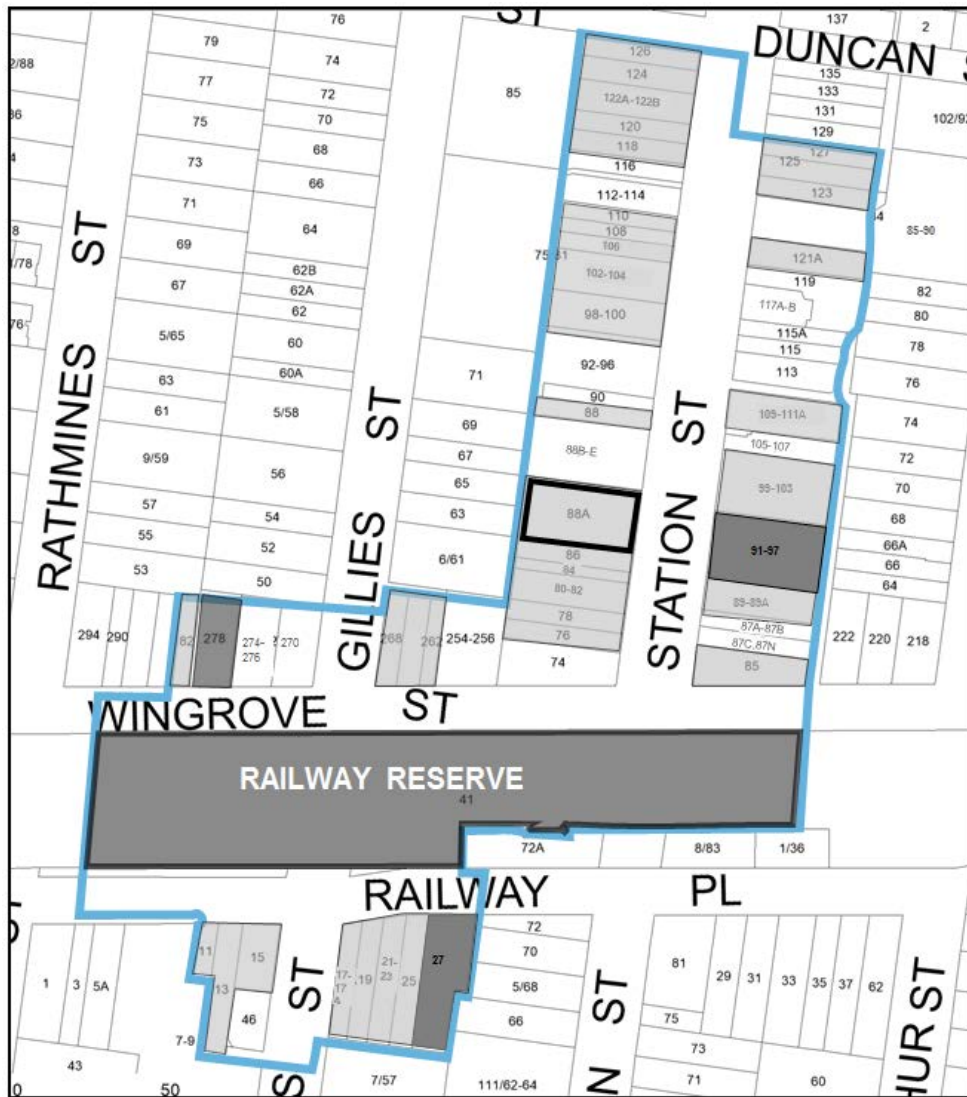


## Rossmoyne Street Precinct, Thornbury








## Woolton Avenue Precinct, Thornbury





## Fairfield Village Heritage Precinct, Fairfield.

KEY	
<b>Fairfield Village Heritage Precinct.</b>	
	Proposed Heritage Overlay Precinct boundary.
	Location of existing Heritage Overlay within the proposed precinct. HO190 church.
	Location of <i>Significant</i> heritage places within the proposed precinct.
	Location of <i>Contributory</i> heritage places within the proposed precinct.
	Location of not significant/not contributory places.